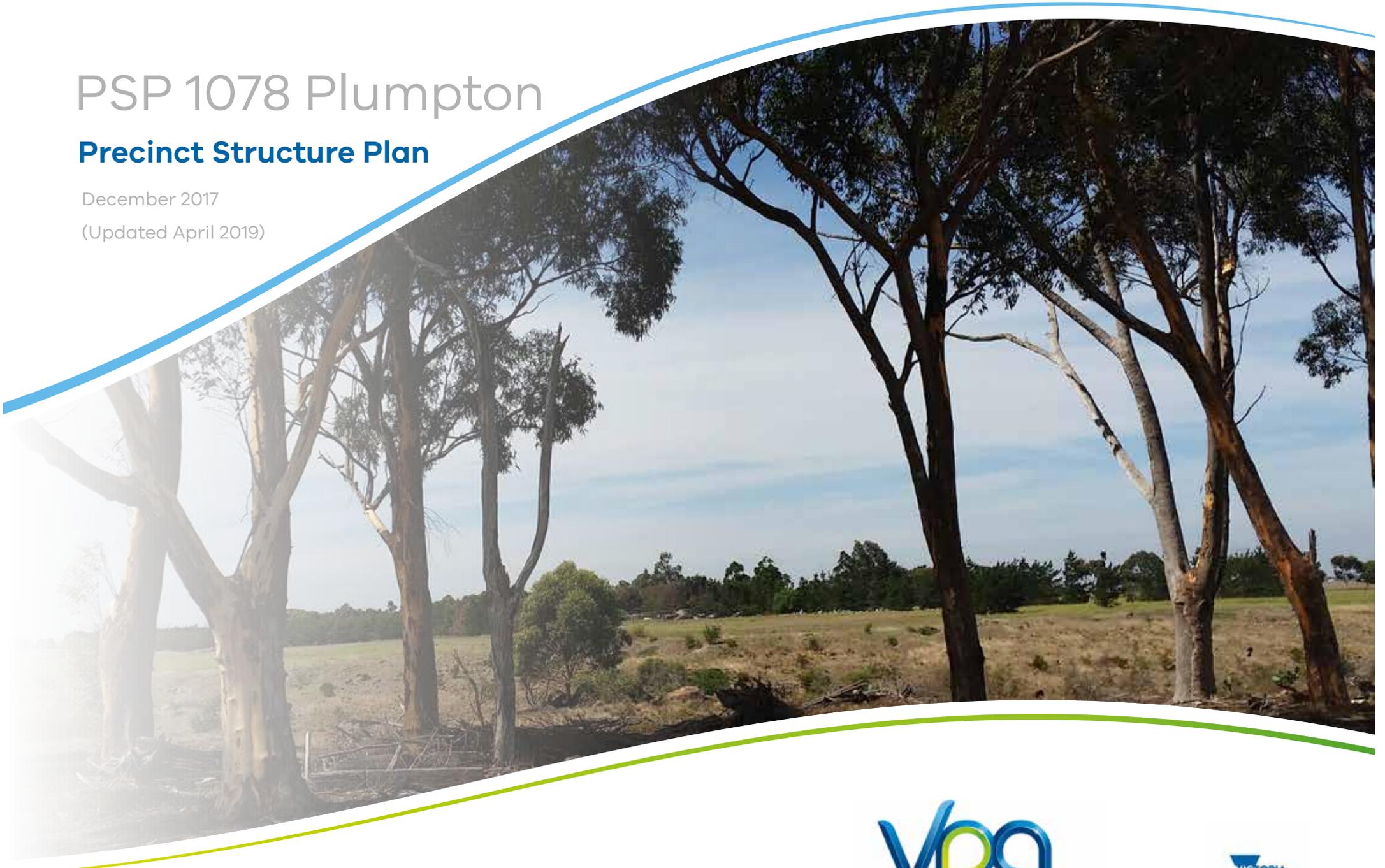


# PSP 1078 Plumpton

## Precinct Structure Plan

December 2017

(Updated April 2019)



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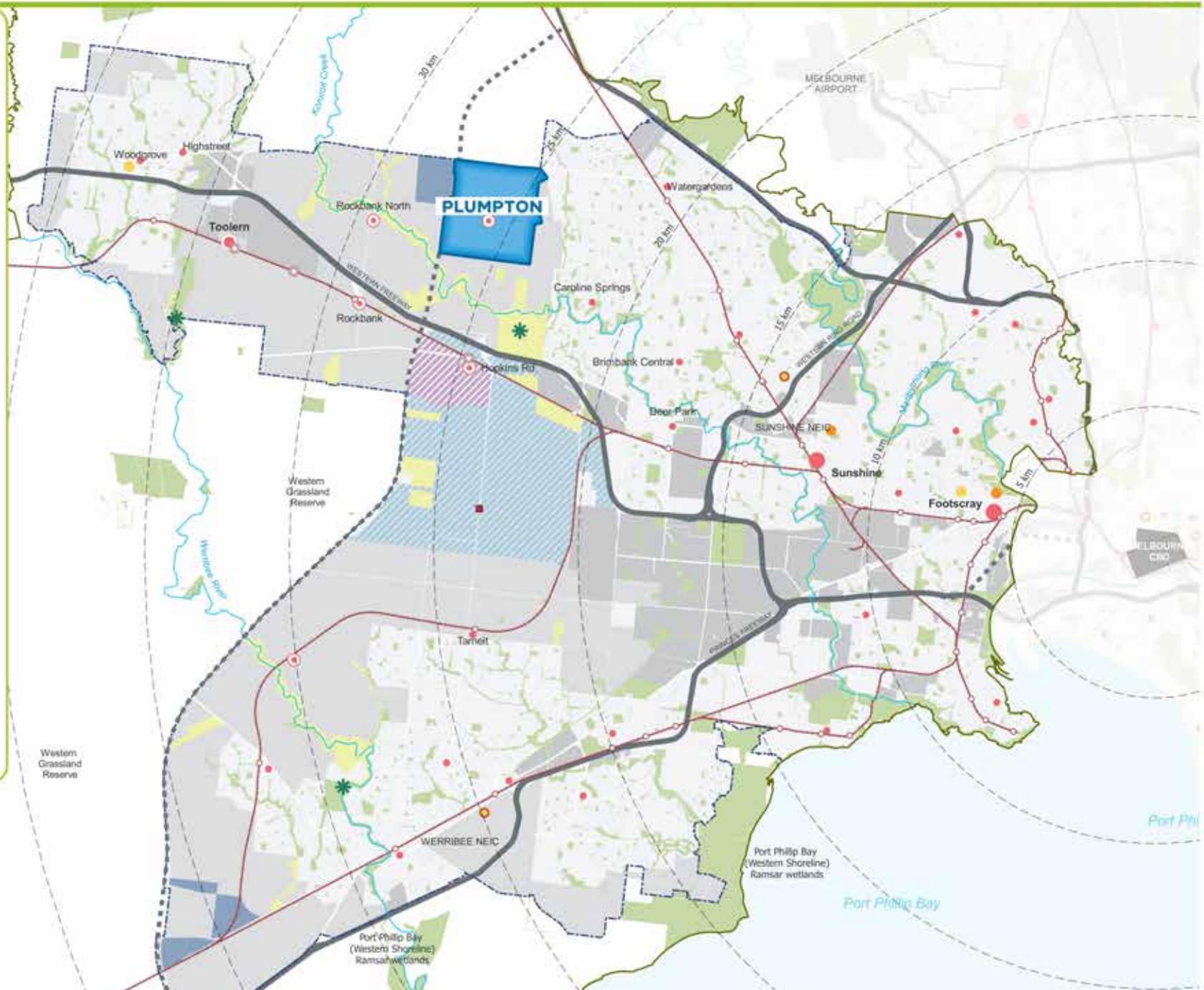
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-  western region boundary
  -  urban growth boundary
  -  urban area
  -  growth area
  -  state-significant industrial precinct - future
  -  business and residential precinct
  -  industrial area - future
  -  industrial area - existing
  -  public open space
  -  conservation
  -  regional park - emerging and proposed
  -  waterway
  -  metropolitan activity centre - existing
  -  metropolitan activity centre - future
  -  activity centre - existing
  -  activity centre - future
  -  education precinct
  -  health precinct
  -  health and education precinct
  -  state significant road corridor
  -  transport projects - committed and potential future
  -  rail network
  -  interstate freight terminal (indicative)
  -  train station - existing
  -  train station - proposed
- Adapted from source: Plan Melbourne 2017 - 2050 (Department of Environment, Land, Water and Planning, 2017)



## 1.0 INTRODUCTION

The Plumpton Precinct Structure Plan (the PSP) has been prepared by the Victorian Planning Authority (VPA) in consultation with Melton City Council and with the assistance of Government agencies, service authorities and major stakeholders.

A PSP is a long-term plan for urban development. It describes how the land is expected to be developed, and how and where services are planned to support development.

This PSP guides proposed development within the Plumpton Precinct (the precinct).

Generally, the PSP:

- Sets out plans to guide the delivery of quality urban environments in accordance with relevant Victorian Government guidelines, including the *Precinct Structure Planning Guidelines*, the *Victorian Planning and Environment Act, 1987* and the State Planning Policy Framework;
- Enables the transition of non-urban to urban land;
- Sets the vision for how land should be developed and the outcomes achieved;
- Outlines the projects required to ensure that future residents, visitors and workers within the area can be provided with timely access to services and transport necessary to support a quality and affordable lifestyle;
- Sets out objectives, guidelines and requirements for land use and development;
- Provides Government agencies, the Council, developers, investors and local communities certainty about future development;
- Addresses the requirements of the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act 1999) in accordance with an endorsed program under Part 10\*; and
- Development must also comply with other Acts and approvals where relevant e.g. in the case of Aboriginal cultural heritage, compliance with the *Aboriginal Heritage Act 2006* is required.

The PSP is informed by:

- The State and Local Planning Policy Framework set out in the Melton Planning Scheme;
- *Precinct Structure Planning Guidelines* (Growth Areas Authority, 2009);
- *Growth Corridor Plans: Managing Melbourne's Growth Areas* (Growth Areas Authority, 2012);
- *Biodiversity Conservation Strategy for Melbourne's Growth Corridors (BCS) and Sub Regional Species Strategies for Melbourne's Growth Corridors* (Department of Environment and Primary Industries, June 2013)\*; and
- *Plan Melbourne 2017 - 2050* (Victorian Government, 2017).

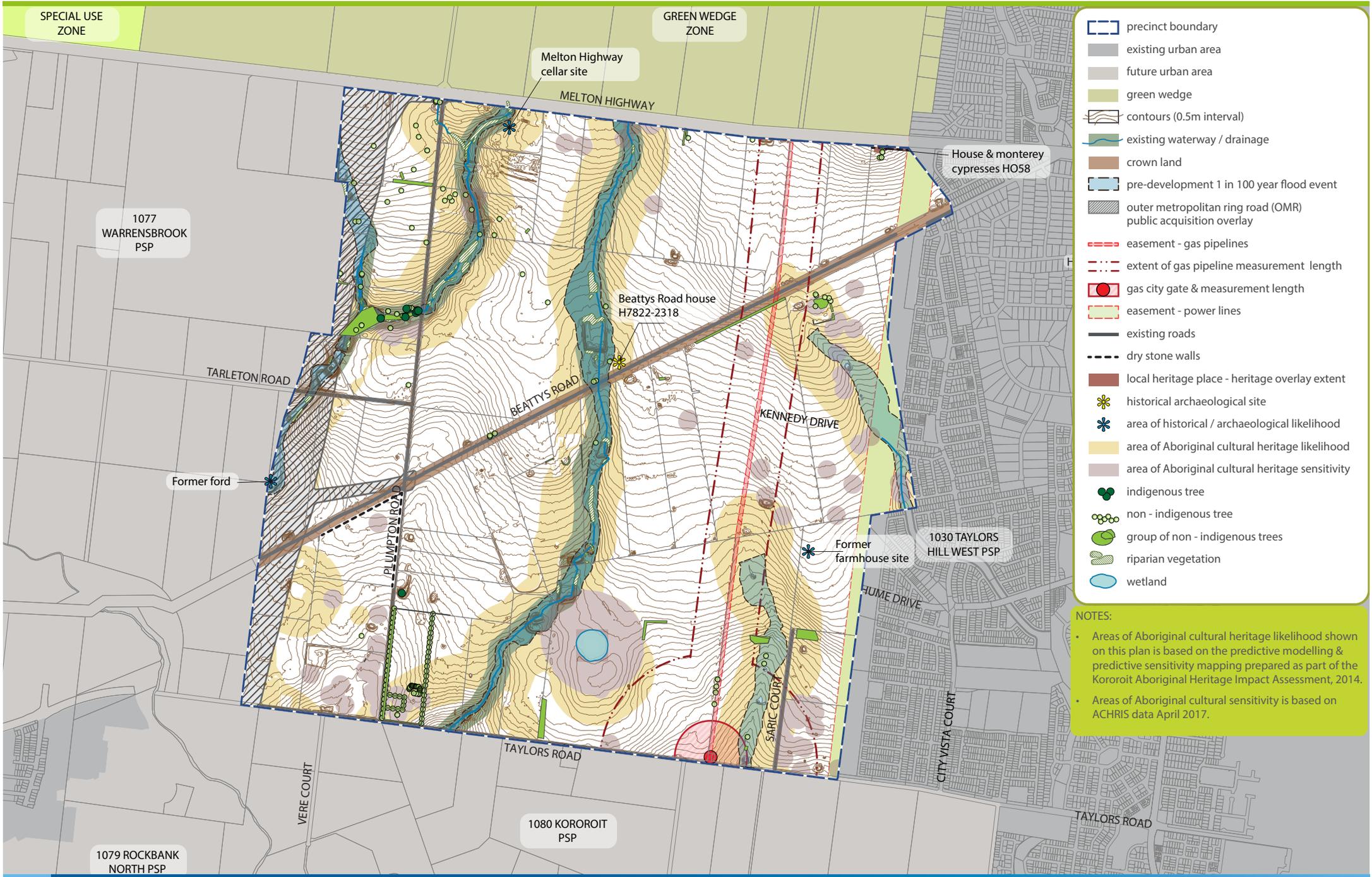
The following documents have been developed in parallel with the PSP to inform and direct the future planning and development of the precinct:

- *PSP 1078 Plumpton and PSP 1080 Kororoit Background Report* (2017); and
- *Plumpton and Kororoit Infrastructure Contributions Plan* (2017)

\*On 5 September 2013 an approval under the Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act) was issued by the Commonwealth Minister for Environment, Heritage and Water. The approval applies to all actions associated with urban development in growth corridors in the expanded Melbourne 2010 Urban Growth Boundary as described in page 4 in the Biodiversity Conservation Strategy for Melbourne's Growth Corridors (Department of Environment and Primary Industries, 2013). The Commonwealth approval has effect until 31 December 2060. The approval is subject to conditions specified at Annexure 1 of the approval.

This includes the payment of habitat compensation obligations to the Department of Environment, Land, Water and Planning (DELWP) and the restriction of urban development in conservation areas. The habitat compensation obligations for land parcels located within the Melbourne Strategic Assessment program area can be estimated using the DELWP Native Vegetation Information Management (NVIM) system, available at <https://nvim.delwp.vic.gov.au/BCS>. Requests to meet the habitat compensation obligations for a project/development are made by registering through the NVIM portal.

Provided the conditions of the EPBC Act approval are satisfied, individual assessment and approval under the EPBC Act is not required.



- precinct boundary
- existing urban area
- future urban area
- green wedge
- contours (0.5m interval)
- existing waterway / drainage
- crown land
- pre-development 1 in 100 year flood event
- outer metropolitan ring road (OMR) public acquisition overlay
- easement - gas pipelines
- extent of gas pipeline measurement length
- gas city gate & measurement length
- easement - power lines
- existing roads
- dry stone walls
- local heritage place - heritage overlay extent
- historical archaeological site
- area of historical / archaeological likelihood
- area of Aboriginal cultural heritage likelihood
- area of Aboriginal cultural heritage sensitivity
- indigenous tree
- non - indigenous tree
- group of non - indigenous trees
- riparian vegetation
- wetland

**NOTES:**

- Areas of Aboriginal cultural heritage likelihood shown on this plan is based on the predictive modelling & predictive sensitivity mapping prepared as part of the Kororoit Aboriginal Heritage Impact Assessment, 2014.
- Areas of Aboriginal cultural heritage sensitivity is based on ACHRIS data April 2017.

## 1.1 How to read this document

The Plumpton PSP guides land use and development where a planning permit is required under the Urban Growth Zone or another zone where that zone references this PSP.

A planning application and planning permit must implement the outcomes of the PSP. The outcomes are expressed as the vision and objectives.

Each element of the PSP contains requirements and guidelines as relevant.

**Requirements** must be adhered to in developing the land. Where they are not demonstrated in a permit application, requirements will usually be included as a condition on a planning permit whether or not they take the same wording as in this precinct structure plan. A requirement may include or reference a plan, table or figure in the precinct structure plan.

**Guidelines** express how discretion will be exercised by the responsible authority in certain matters that require a planning permit. If the responsible authority is satisfied that an application for an alternative to a guideline implements the outcomes the responsible authority may consider the alternative. A guideline may include or reference a plan, table or figure in the precinct structure plan.

Meeting these requirements and guidelines will implement the outcomes of the PSP.

Development must also comply with other Acts and approvals where relevant.

E.g. the *Environment Protection and Biodiversity Conservation Act 1999* in the case of biodiversity or the *Aboriginal Heritage Act 2006* in the case of cultural heritage amongst others.

Not every aspect of the land's use and development is addressed in this structure plan and a responsible authority may manage development and issue permits as relevant under its general discretion.

## 1.2 Land to which the Precinct Structure Plan applies

Plumpton (PSP 1078) covers an area of 1,016 hectares and located approximately 30 kilometres to the west of the Melbourne CBD. The precinct is bounded by Melton Highway to the north, the approved Taylors Hill West PSP to the east, Taylors Road and the Kororoit PSP to the south and the Outer Metropolitan Ring road (OMR) reservation, the approved Rockbank North PSP and future Warrensbrook PSP to the West. The Plumpton Precinct is illustrated on Plan 2.

## 1.3 Plumpton & Kororoit Infrastructure Contributions Plan (ICP)

The Plumpton and Kororoit ICP sets out the requirements for development proponents to contribute towards basic and essential infrastructure required to support development of the precinct. The ICP is a separate document incorporated into the Melton Planning Scheme and implemented through Schedule 1 to Clause 45.10 of the Melton Planning Scheme.

**Table 8** - Precinct Infrastructure identifies which infrastructure projects are to be funded through the ICP.

## 1.4 Background Information

Background information on the precinct including its local and metropolitan context, history, landform and topography, biodiversity, drainage, open space, transport and community facilities is provided in the separate *PSP 1078 Plumpton and PSP 1080 Kororoit Background Report*. This report also references the various background technical studies that have informed preparation of the precinct structure plan.



- precinct boundary
- walkable residential catchment
- residential
- business
- mixed use
- industrial
- town centre
- local convenience centre
- future government school
- potential non-government school
- community facilities
- indoor recreation (aquatic centre)
- uncredited open space (drainage)
- credited open space
- uncredited open space (beattys road reserve)
- easement with pylons - utilities
- proposed substation
- public acquisition overlay
- arterial road (6 lanes)
- arterial road (4 lanes)
- connector street
- connector street - town centre
- feature main street
- local access street
- future premium bus route
- existing urban area
- future urban area
- local heritage place
- pedestrian & cyclist bridge

**NOTE:**

- For open space details see plan 6
- For local street hierarchy see plan 8
- Local road alignments illustrated on this plan and plan 8 are indicative only. Detailed planning at the subdivision stage is envisaged to identify actual road alignments and access arrangements (including Beattys road), which should generally accord with the indicative alignments shown in the PSP, subject to the responsible authority's approval.
- Areas of the PSP are subject to flooding in the pre-development 1 in 100 year flood extent. see plan 2 for flood extent.

## 2.0 OUTCOMES

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### 2.1 Vision

The Plumpton Precinct will provide over 12,700 jobs close to where people live, with synergies between the Major Town Centre and adjacent commercial and industrial areas encouraging a diversity of employment. The library, council facilities and nearby aquatic centre will complement the retail and commercial offer in the Major Town Centre, with a thriving café/ entertainment area making the most of the outlook onto the adjacent waterway.

Excellent transport connections along a connector and arterial road network which leverages existing roads and road reservations will link workers and residents to existing rail stations along the Melton and Sunbury rail corridors and to a possible future station at Mt Atkinson, as well as to the Melton Highway and the future Outer Metro Ring. Walking and cycling to town centres, schools and parks will be the modes of choice along tree-lined streets with dedicated pedestrian and cycle paths.

The erstwhile gold route along Beattys Road shapes the structure of the precinct and will be reinterpreted as a street and linear park. Beattys Road Reserve will have strategic significance and function as a beautified 'civic spine' to draw visitors to play areas and community facilities punctuating its length and will provide strong links to areas east and west of the PSP. North-south easements and waterways will be popular recreation areas which connect through to the Kororoit Creek, the proposed Kororoit Regional Park and the historic Deanside Homestead Complex in the Kororoit PSP area to the south. Linear paths along this open space network will also connect to local destinations including parks, play areas and sporting reserves which will encourage healthy lifestyles and engagement across the community. Sporting reserves will be developed in a range of sizes to adapt to different sporting needs over time and are located near to waterways so that stormwater harvesting may be realised in future.

The local Aboriginal history of the area will be celebrated through protection of significant places and through signage and information relating to this history. The area's post-contact history will be remembered through retention of dry stone walls and other heritage sites, which will help develop a sense of place for the growing community.

Diversity of dwelling sizes and types will provide affordable and flexible housing and live-work opportunities. Both the Major Town Centre and the Local Town Centre will offer shopping, community facilities and services immediately adjacent to residential areas, providing convenient access over the full life-cycle to enable ageing-in-place. The adjacency of higher density residential areas to the town centres will ensure that these are vibrant places by day and by night.

## 2.2 Key Objectives

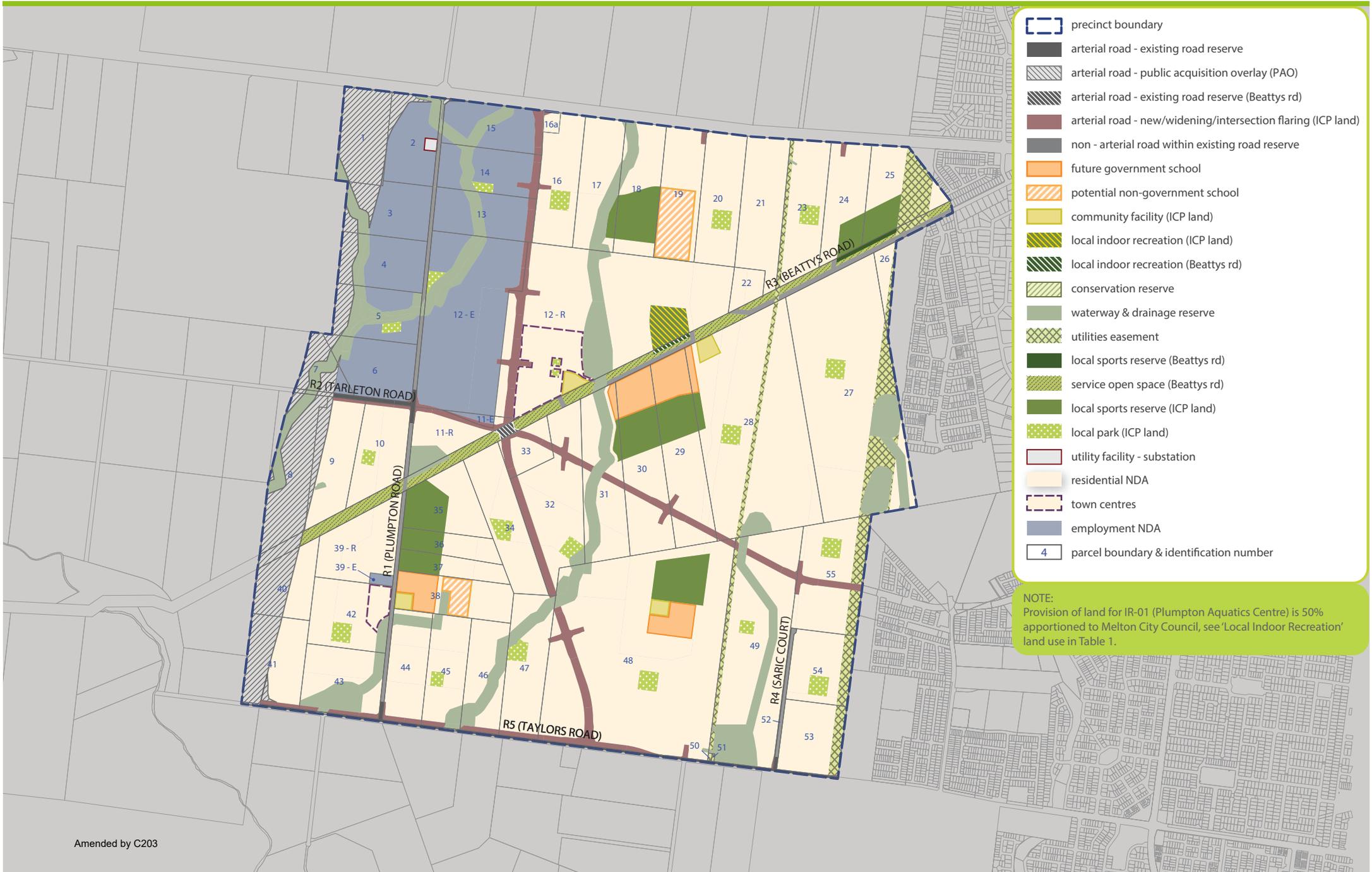
The development of the Plumpton PSP area is guided by a set of key development objectives.

Development within Plumpton will seek to:

OBJECTIVES	
IMAGE, CHARACTER, HERITAGE & HOUSING	
<b>01</b>	Deliver a minimum of 10,700 new homes across the precinct and promote increased housing choice and density within a walkable catchment of high amenity features and public transport.
<b>02</b>	Identify, retain and celebrate Aboriginal cultural heritage places within the precinct.
<b>03</b>	Encourage a strong sense of place through the protection, enhancement and interpretation of places of post-contact cultural heritage significance, in particular Beattys Road former goldfields route, Melton Highway House (HO68) and dry stone walls.
<b>04</b>	Deliver a high quality landscaped interface between residential areas and the Plumpton Business and Industrial Precinct bordered by Hopkins and Tarletons Roads, to minimise potential impacts of industrial uses on residential amenity, and to ensure viability of industrial and commercial land uses.
EMPLOYMENT AND TOWN CENTRES	
<b>05</b>	Deliver over 12,750 local jobs through capitalising on the significant opportunities of the local context including the growing residential population; existing and proposed roads, including the future extension of Hopkins Road which will connect the Princes Highway to the Melton Highway (and possibly through to Sunbury in future); the future Outer Metropolitan Ring; the possible future rail station at Mt Atkinson; and connections to the future industrial area in Warrensbrook PSP.
<b>06</b>	Attract a diversity of businesses and employers to Plumpton and in particular to the Business and Industrial Precinct; the Major Town Centre; and the Local Town Centre, through creating a range of lot sizes which respond to local conditions including topography, environmental features and the street network.
<b>07</b>	Develop the Major Town Centre at the 'pinwheel' intersection of Beattys, Hopkins and Tarletons Roads, using the connections of Beattys Road to maximise the catchment to the Town Centre, drawing visitation from the broader communities of Plumpton and Kororoit PSP and beyond.
<b>08</b>	Ensure the Major Town Centre and the Local Town Centre deliver high quality public spaces and civic uses as well as retail and commercial uses and attract visitors through clear identification at gateway sites along Plumpton, Hopkins and Beattys Roads as well as connector streets.

<b>09</b>	Develop a high quality interface between the Plumpton Business and Industrial Precinct and the Plumpton Major Town Centre to maximise connections and synergies between the two areas.
<b>010</b>	Deliver high amenity town centres by prioritising access for walking, cycling and public transport.
<b>011</b>	Encourage lower-cost, flexible space in and adjacent to the Major Town Centre and Local Town Centre for a range of small local enterprises to ensure these centres have an ability to adapt and evolve over time.
<b>012</b>	Encourage the provision of a Local Convenience Centre in the south east of the precinct and in the Business and Industrial Precinct without compromising the functions and roles of nearby town centres.
OPEN SPACE AND COMMUNITY FACILITIES	
<b>013</b>	Develop an open space network which connects to local and regional destinations including the proposed Kororoit Regional Park, the Kororoit Creek, the Mt Atkinson volcanic cone area and the Western Grasslands Reserve.
<b>014</b>	Encourage walking, cycling and other recreation opportunities by providing connections between the various elements of the open space network including along streets, local parks, sports reserves, public plazas, waterways, gas and powerlines easements, and the historic Beattys Road goldfields route.
<b>015</b>	Retain the street function of Beattys Road reserve while delivering a continuous linear park function along its length, with development fronting the reserve and maximising use of the linear park for walking, cycling, play, community facilities and as a direct connection to the Major Town Centre.
<b>016</b>	Maximise the open space benefits of the powerlines and gas easements, which run north-south through the precinct, through developing a shared path within each, and complemented by indigenous design landscaping and recreational uses within the easements.
<b>017</b>	Provide for government and non-government school sites to meet the strategically justified need for State and non-government education in the area.
<b>018</b>	Ensure the health and wellbeing of residents is protected by delivering a built environment of facilities and amenities that promote healthy lifestyle practices, social interaction, civic engagement and access to services.
<b>019</b>	Develop a walkable network of community hubs that provide access to social, education, recreation and health services within each neighbourhood.
BIODIVERSITY, THREATENED SPECIES AND BUSHFIRE MANAGEMENT	
<b>020</b>	Contribute to the long term conservation of significant flora and fauna species through protection of habitat, particularly along waterways which flow into the Kororoit Creek.
<b>021</b>	Ensure that bushfire hazards are identified and that protection measures are considered in the layout and design of the local street network, subdivisions and buildings and works.

TRANSPORT AND MOVEMENT	
<b>O22</b>	Provide safe, continuous and inviting paths of travel for pedestrians and cyclists to key destinations and trails, including the Major Town Centre and Local Town Centre; schools and community hubs; shared paths along waterways and easements; and to the Kororoit Creek and proposed Kororoit Regional Park to the south.
<b>O23</b>	Create a clear and legible street network that provides straightforward connections to the wider public transport and road network.
<b>O24</b>	Maximise use of public transport by providing an efficient bus-capable road network that services key destinations throughout the precinct.
INTEGRATED WATER MANAGEMENT AND UTILITIES	
<b>O25</b>	Deliver an integrated water management system that reduces reliance on reticulated potable water, increases the re-use of alternative water, responds to local soil types, minimises flood risk, ensures waterway health, and contributes towards a sustainable and green urban environment.
<b>O26</b>	Ensure sensitive land uses are located outside the measurement length of the high pressure gas transmission pipelines where possible and that construction is managed to minimise risk of any adverse impacts.
PRECINCT INFRASTRUCTURE PLAN AND STAGING	
<b>O27</b>	Deliver Hopkins Road and associated waterway crossings early in the staging of the precinct, to open up access to the Major Town Centre and the Business and Industrial Precinct, and to reduce pressure on existing and proposed connector streets.
<b>O28</b>	Provide all lots with potable water, electricity, reticulated sewerage, drainage, gas and telecommunications to the satisfaction of the relevant authority.
<b>O29</b>	Deliver cohesive and integrated neighbourhoods by co-ordinating development with the delivery of key local and state infrastructure.



- precinct boundary
- arterial road - existing road reserve
- arterial road - public acquisition overlay (PAO)
- arterial road - existing road reserve (Beattys rd)
- arterial road - new/widening/intersection flaring (ICP land)
- non - arterial road within existing road reserve
- future government school
- potential non-government school
- community facility (ICP land)
- local indoor recreation (ICP land)
- local indoor recreation (Beattys rd)
- conservation reserve
- waterway & drainage reserve
- utilities easement
- local sports reserve (Beattys rd)
- service open space (Beattys rd)
- local sports reserve (ICP land)
- local park (ICP land)
- utility facility - substation
- residential NDA
- town centres
- employment NDA
- parcel boundary & identification number

**NOTE:**  
Provision of land for IR-01 (Plumpton Aquatics Centre) is 50% apportioned to Melton City Council, see 'Local Indoor Recreation' land use in Table 1.

Amended by C203

## 2.3 Summary Land Use Budget

The Plumpton PSP land use budget in Table 1 provides a summary of the land required for transport, community facilities, government education facilities, and open space and identifies the total amount of land available for development.

The Net Developable Area (NDA) is established by deducting the land requirements for transport, community facilities, public and private education facilities, open space (sports reserves and local parks), drainage corridors, conservation areas and other encumbered land from the Gross Developable Area (GDA).

The GDA for the Plumpton precinct is 1016.09 hectares, with a total NDA of 705.47 hectares. The residential NDA is 598.91 hectares, meaning approximately 58.94% of the land within the Plumpton PSP area is available for residential development, while 106.57 hectares, or 10.49% of the land, is available for predominantly employment uses.

Based on the estimated residential development yield established in Table 3 – Housing Density Guide, Plumpton PSP will generate at least 10,700 dwellings to accommodate over 30,100 residents.

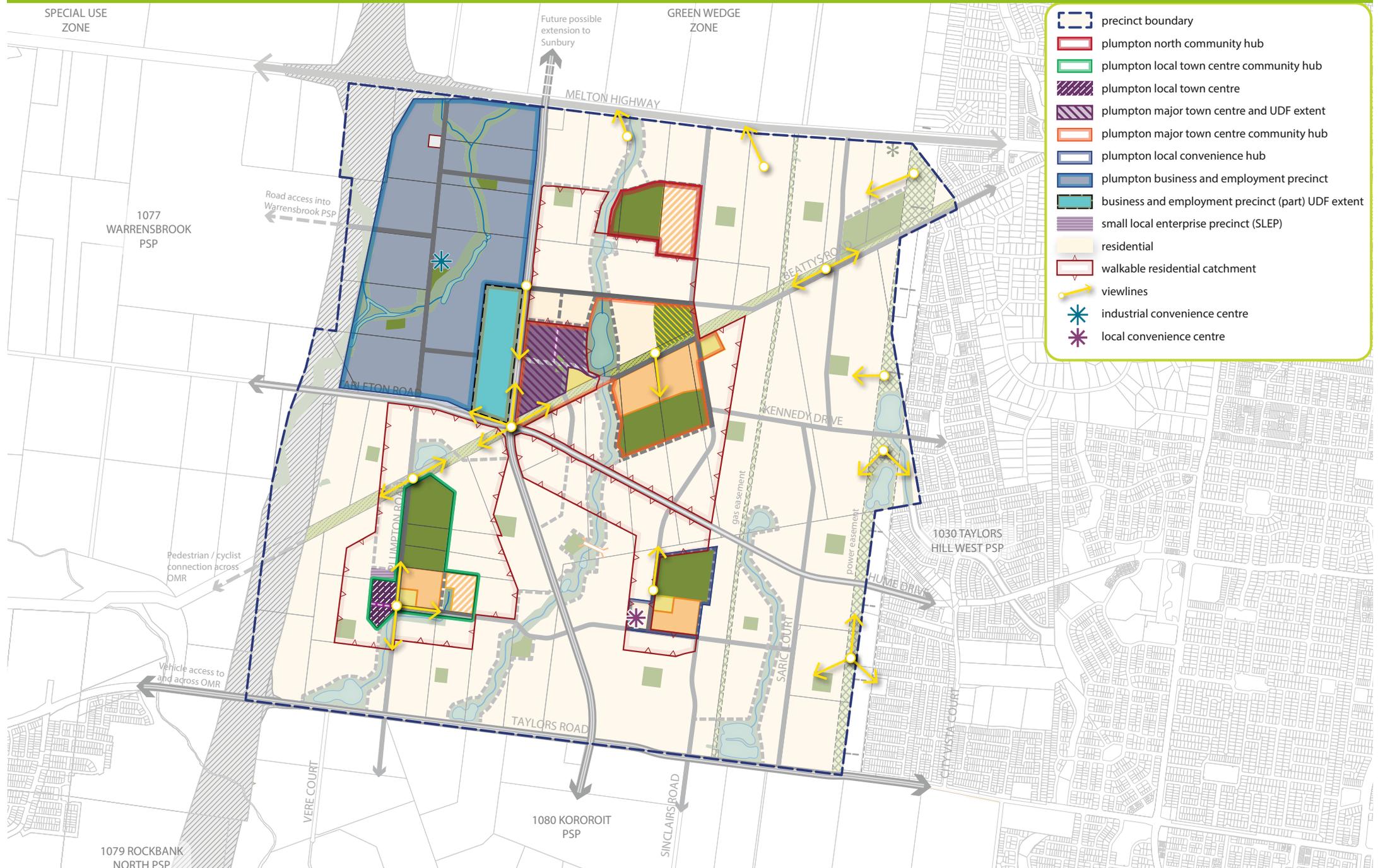
Amended by C203 **Table 1** Summary land use budget

DESCRIPTION	PSP 1078		
	HECTARES	% OF TOTAL	% OF NDA
<b>TOTAL PRECINCT AREA (HA)</b>	<b>1,016.09</b>		
<b>TRANSPORT</b>			
Arterial Road - Existing Road Reserve	4.36	0.43%	0.62%
Arterial Road - Public Acquisition Overlay	52.31	5.15%	7.42%
Arterial Road - Existing Road Reserve (Beattys Road)	0.56	0.05%	0.08%
Arterial Road - New / Widening / Intersection Flaring (ICP land)	31.68	3.12%	4.49%
Non-Arterial Road - Retained Existing Road Reserve	11.59	1.14%	1.64%
Sub-total Transport	100.50	9.9%	14.25%
<b>COMMUNITY &amp; EDUCATION</b>			
Future Government School	17.00	1.67%	2.41%
Potential Non-Government School	9.60	0.94%	1.36%
Local Community Facility (ICP land)	3.81	0.38%	0.54%
Local Indoor Recreation (ICP land)	1.80	0.18%	0.26%
Local Indoor Recreation	1.80	0.18%	0.26%
Local Indoor Recreation (Beattys Road)	0.43	0.04%	0.06%
Sub-total Community & Education	34.44	3.4%	4.9%
<b>OPEN SPACE</b>			
<b>UNCREDITED OPEN SPACE</b>			
Waterway and Drainage Reserve	81.04	7.98%	11.49%
Utilities Easement	25.57	2.52%	3.62%
Local Sports Reserve (Beattys Road)	0.71	0.07%	0.10%
Service Open Space (Beattys Road)	14.66	1.44%	2.08%
Sub-total Uncredited Open Space	121.98	12.01%	17.29%
<b>CREDITED OPEN SPACE</b>			
Local Sports Reserve (ICP land)	38.29	3.77%	5.43%
Local Park (ICP land)	15.00	1.48%	2.13%
Sub-total Credited Open Space	53.29	5.24%	7.55%
Total All Open Space	175.27	17.25%	24.84%

DESCRIPTION	PSP 1078		
	HECTARES	% OF TOTAL	% OF NDA
<b>OTHER</b>			
Utilities Sub-stations / facilities (acquired by relevant authority)	0.40	0.04%	0.06%
Sub-total	0.40	0.04%	0.06%
<b>TOTAL NET DEVELOPABLE AREA (NDA) HA</b>	<b>705.48</b>	<b>69.43%</b>	
<b>NET DEVELOPABLE AREA - RESIDENTIAL (NDAR) HA</b>	<b>598.91</b>	<b>58.94%</b>	
<b>NET DEVELOPABLE AREA - EMPLOYMENT (NDAE) HA</b>	<b>106.57</b>	<b>10.49%</b>	

RESIDENTIAL LOCAL OPEN SPACE (EXPRESSED AS % OF NDAR)	HECTARES	% OF NDAR
Local Sports Reserve (ICP land)	38.29	6.39%
Local Park (ICP land)	13.50	2.25%
Sub-total	51.79	8.65%
EMPLOYMENT LOCAL OPEN SPACE (EXPRESSED AS % OF NDAE)	HECTARES	% OF NDAE
Local Park (ICP land)	1.50	1.41%
Sub-total	1.50	1.41%
<b>Total Open Space</b>	<b>53.29</b>	<b>7.55%</b>

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## 3.0 IMPLEMENTATION

### 3.1 Image, character, heritage & housing

#### 3.1.1 Image and character

REQUIREMENTS	
<b>R1</b>	All public landscaped areas must be planted and designed to the satisfaction of the responsible authority.
<b>R2</b>	Street trees must be planted on both sides of all roads and streets (excluding laneways) at regular intervals appropriate to tree size at maturity, in accordance with relevant Council landscaping policy unless otherwise agreed by the responsible authority.
<b>R3</b>	Street tree planting on declared arterial roads must be established in accordance with the clear zone guidelines to the satisfaction of the coordinating road authority.
<b>R4</b>	Trees in parks and streets must be: <ul style="list-style-type: none"> <li>• Suitable for local conditions;</li> <li>• Planted in modified and improved soil to support tree establishment and longevity; and</li> <li>• Consistent with any guidance provided on the relevant cross section within this PSP unless otherwise approved by the responsible authority.</li> </ul>
<b>R5</b>	Boundary fences forward of the building line must not exceed 1.2 metres in height.
GUIDELINES	
<b>G1</b>	Streets should be provided directly abutting waterway reserves, open spaces and utilities easements to ensure houses generally face these public spaces.
<b>G2</b>	In locations where the responsible authority is satisfied it is not feasible to locate a street adjacent to the open space network (including waterway reserve, open space or utilities easement), then houses should face the path within the open space network path and be 'rear-loaded'.
<b>G3</b>	High quality landscape treatments should be provided throughout the precinct, most particularly in streetscapes, local parks and along creek and drainage waterway corridors.
<b>G4</b>	Street networks within subdivisions should be designed to maximise the number of connections and direct views to the open space network and town centres.
<b>G5</b>	Subdivision design should incorporate natural and built design elements which respond to local heritage and topography to assist in place making and the achievement of a sense of place.

<b>G6</b>	To reinforce neighbourhood character and the role of the street or public place, a consistent suite of lighting and furniture should be used across neighbourhoods, appropriate to the type and role of street or public space, unless otherwise approved by the responsible authority.
<b>G7</b>	Salvaged rocks should be retained on site where possible and incorporated in the design of waterways, retaining structures, fences and other landscape features.
<b>G8</b>	Existing trees shown on Plan 2 should be retained where possible along streets and in subdivisions.
<b>G9</b>	Built form on corner lots should provide a positive address to both frontages. This can be achieved through appropriate use of glazing, and other architectural treatments.
<b>G10</b>	Built form should add to the precinct character by providing an attractive street address that encourages passive surveillance and visual interest.
<b>G11</b>	Sites in prominent locations, such as Plumpton Major Town Centre and major intersections, should be developed to respond to their strategic location and preferably have greater height, density and architectural quality subject to limitations imposed by utilities or external land uses (refer Appendix B).

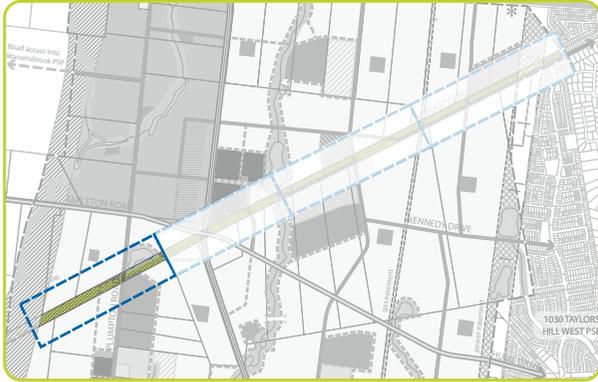
### 3.1.2 Heritage

REQUIREMENTS	
<b>R6</b>	Any subdivision and/or development of land adjoining a heritage site identified under the Heritage Overlay in the Melton Planning Scheme and/or of post-contact cultural heritage significance, must have regard to the heritage significance of the site and provide a sensitive interface.
<b>R7</b>	Beattys Road Reserve must be retained for public use with connector road and local road functions as per concept plans in Figure 1 to Figure 4 and as per Plan 8 and delivered by relevant adjacent property as per Table 9 in Appendix G, with a continuous off-road shared path, and community uses at designated locations and dry stone walls retained as required by this PSP, to the satisfaction of the responsible authority.
<b>R8</b>	<p>Dry stone walls illustrated on Plan 2 must be retained unless otherwise agreed by the responsible authority. Dry stone walls to be retained must:</p> <ul style="list-style-type: none"> <li>• Be situated within public open space or road reserve to the satisfaction of the responsible authority;</li> <li>• Have a suitable landscape interface;</li> <li>• Be checked and repaired by a suitably qualified dry stone waller for any loose stones. Any loose stones are to be reinstated in the wall in secure positions;</li> <li>• Retain post and wire or post and rail fences situated within the walls, with any wire protruding beyond the vertical face of the wall reinstated to its original position or removed; and</li> <li>• Be incorporated into subdivision design to minimise disturbance to the walls (e.g. utilisation of existing openings for vehicle and pedestrian access).</li> </ul>
<b>R9</b>	Any reinstatement or repair of walls must be undertaken by a suitably qualified dry stone waller and is to be consistent with the construction style of the original wall.
<b>R10</b>	Installation of services across the alignment of retained dry stone walls must be undertaken by boring rather than open trenching. If open trenching or disturbance to the wall is unavoidable, a minimum section of wall may be temporarily removed and then reinstated to original condition under the supervision of a suitably qualified dry stone waller to the satisfaction of the responsible authority.

<b>R11</b>	<p>Reinstatement of walls must use stone from (in order of priority):</p> <ul style="list-style-type: none"> <li>• The original wall in that location (including fallen stone adjacent to the wall)</li> <li>• A nearby section of the wall approved to be removed</li> <li>• Any adjacent paddock containing wall parts which can be recovered</li> <li>• Walls approved to be removed in the nearby area (including any stone which has been stockpiled by Council).</li> </ul>
<b>R12</b>	Housing and other development must front Beattys Road Reserve unless agreed by the responsible authority.

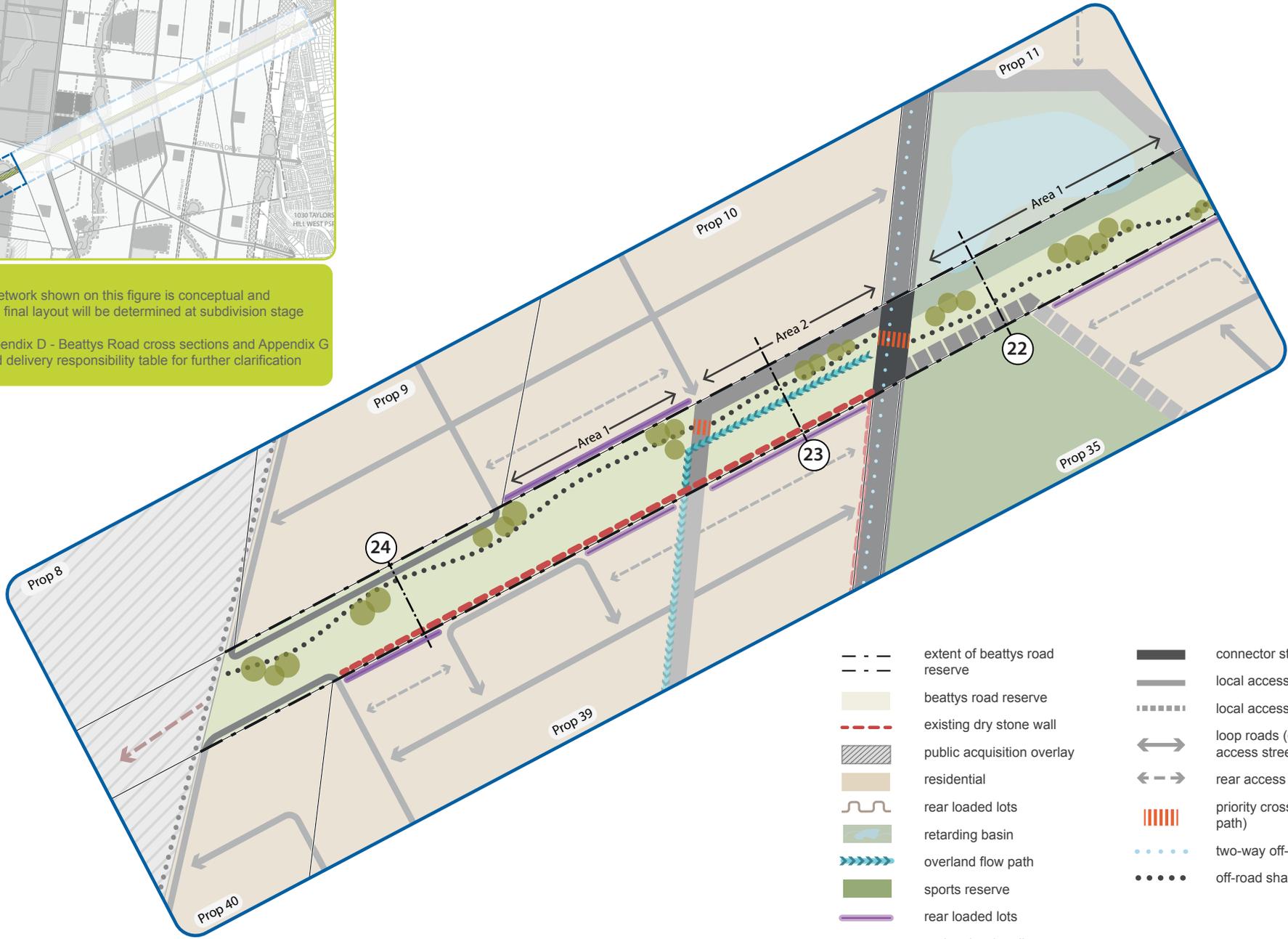
GUIDELINES	
<b>G12</b>	Identify opportunities for interpretation of local history and values within the Beattys Road Reserve.
<b>G13</b>	Development of land subject to the Heritage Overlay in the Melton Planning Scheme should ensure that the heritage place is recognised within, and well integrated with, the subdivision.
<b>G14</b>	Beattys Road Reserve should be delivered as per the indicative concept plans (Figure 1 to Figure 4) and as per sections located on Plan 8 and in Appendix D and Appendix G, to the satisfaction of the responsible authority.
<b>G15</b>	Proponents undertaking development of land identified on the Victorian Aboriginal Heritage Register, and/or with high Aboriginal cultural heritage values including those identified on Plan 2, should liaise with the designated Registered Aboriginal Party (or Aboriginal Victoria and Traditional Owner Groups in its absence) to ascertain whether heritage interpretation is appropriate in these identified locations, and how the heritage site(s) should be incorporated into the design of the subdivision.
<b>G16</b>	Land uses abutting retained dry stone walls should enhance public visibility of the walls. Relevant uses include open space, conservation reserve, road verge or property boundary wall.
<b>G17</b>	Where it has been agreed with the responsible authority that an existing dry stone wall is to be removed, land owners should consult with Council to determine whether the stone should be retained for use in repairing other walls within the PSP or landscape designs.
<b>G18</b>	Adaptive reuse of Melton Highway House (HO58) may be appropriate if it is demonstrated that it will contribute to the long term conservation of this heritage place.

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**NOTES**

- The street network shown on this figure is conceptual and indicative. The final layout will be determined at subdivision stage
- Refer to Appendix D - Beattys Road cross sections and Appendix G - Beattys Road delivery responsibility table for further clarification

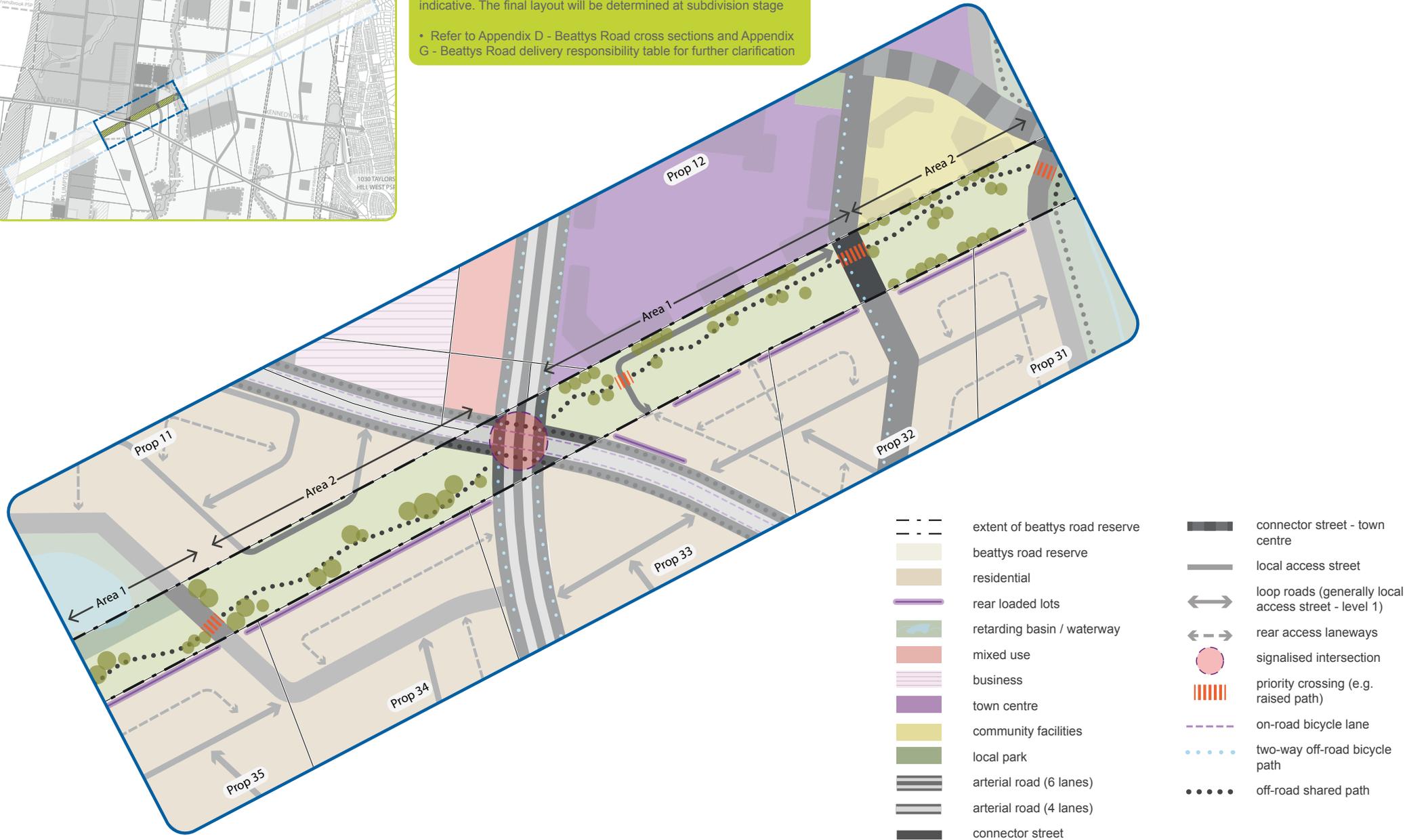


- |                     |  |                         |  |
|---------------------|--|-------------------------|--|
| --- (dashed line)   | extent of beattys road reserve             | — (thick solid line)    | connector street                                     |
| - - - (dotted line) | beattys road reserve                       | — (thin solid line)     | local access street - level 1                        |
| — (solid line)      | existing dry stone wall                    | — (dotted line)         | local access street - level 2                        |
| ▨ (hatched box)     | public acquisition overlay                 | ↔ (double-headed arrow) | loop roads (generally local access street - level 1) |
| ■ (tan box)         | residential                                | ← - - → (dashed arrow)  | rear access laneways                                 |
| ⌒ (wavy line)       | rear loaded lots                           | (vertical bars)         | priority crossing (e.g. raised path)                 |
| 🌊 (blue wavy line)  | retarding basin                            | ⋯⋯⋯ (dotted line)       | two-way off-road bicycle path                        |
| → (blue arrow)      | overland flow path                         | ⋯⋯⋯ (dotted line)       | off-road shared path                                 |
| 🌳 (green box)       | sports reserve                             |                         |  |
| — (purple line)     | rear loaded lots                           |                         |  |
| → (dashed arrow)    | pedestrian / cyclist connection across omr |                         |  |



**NOTES**

- The street network shown on this figure is conceptual and indicative. The final layout will be determined at subdivision stage
- Refer to Appendix D - Beattys Road cross sections and Appendix G - Beattys Road delivery responsibility table for further clarification



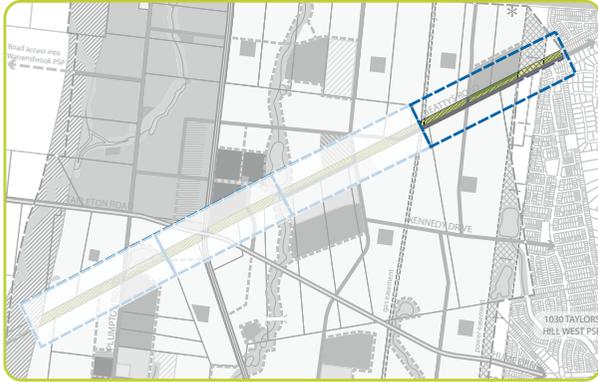


**NOTES**

- The street network shown on this figure is conceptual and indicative. The final layout will be determined at subdivision stage
- Refer to Appendix D - Beattys Road cross sections and Appendix G - Beattys Road delivery responsibility table for further clarification



- |  |                                    |  |  |
|--|------------------------------------|--|--|
|  | extent of beattys road reserve     |  | connector street                                     |
|  | beattys road reserve               |  | connector street - town centre                       |
|  | residential                        |  | local access street                                  |
|  | retarding basin / waterway         |  | loop roads (generally local access street - level 1) |
|  | indoor recreation (aquatic centre) |  | rear access laneways                                 |
|  | government school                  |  | priority crossing (e.g. raised path)                 |
|  | community facilities               |  | two-way off-road bicycle path                        |
|  | easement                           |  | off-road shared path                                 |
|  | rear loaded lots                   |  |  |



**NOTES**

- The street network shown on this figure is conceptual and indicative. The final layout will be determined at subdivision stage
- Refer to Appendix D - Beattys Road cross sections and Appendix G - Beattys Road delivery responsibility table for further clarification



- |  |   |  |  |
|--|---|--|--|
|  | precinct boundary                                   |  | rear access laneways                     |
|  | extent of beattys road reserve                      |  | priority crossing (e.g. raised pavement) |
|  | beattys road reserve                                |  | on-road bicycle lane                     |
|  | residential   |  | two-way off-road bicycle path            |
|  | local sports reserve                                |  | off-road shared path                     |
|  | easement  |  | shared path outside PSP area             |
|  | rear loaded lot                                     |  |  |
|  | connector street                                    |  |  |
|  | local access street                                 |  |  |
|  | loop road (generally local access street - level 1) |  |  |

### 3.1.3 Housing

REQUIREMENTS	
<b>R13</b>	Subdivision of land must deliver an overall minimum average density of 16.5 dwellings per net developable hectare on residential land outside the walkable residential catchment. Where a subdivision proposal represents a single stage or limited number of stages, proponents should demonstrate how the subdivision will contribute to the eventual satisfaction of this guideline through further stages of development.
<b>R14</b>	Subdivision of land within the walkable residential catchment shown on Plan 3 must create lots suitable for the delivery of medium or higher density housing as outlined in Table 2, and also achieve a minimum average density of 21 dwellings per hectare. Applications for subdivision that can demonstrate how target densities can be achieved over time, to the satisfaction of the responsible authority shall be considered.
<b>R15</b>	Dwellings must front or side: <ul style="list-style-type: none"> <li>Waterways and the open space network (including local parks and easements);</li> <li>Arterial and connector streets (including the Outer Metropolitan Ring (refer Appendix D); and</li> <li>The siting of lots to the above must be kept to a minimum.</li> </ul>
<b>R16</b>	Subdivision applications must include layouts for any lots identified for future development of medium density, high density or integrated housing that suitably demonstrate: <ul style="list-style-type: none"> <li>Potential dwelling yield;</li> <li>Active interfaces with adjacent street, open space and waterways;</li> <li>Safe and effective internal vehicle and pedestrian circulation;</li> <li>The delivery of dwelling diversity and lot sizes;</li> <li>Servicing arrangements; and</li> <li>Treatments for sensitive interfaces.</li> </ul>
GUIDELINES	
<b>G19</b>	Residential subdivisions should provide a broad range of lot sizes capable of accommodating a variety of housing types as described in Table 2.
<b>G20</b>	Specialised housing forms such as lifestyle communities, retirement living or aged care facilities should: <ul style="list-style-type: none"> <li>Be integrated into the wider urban structure;</li> <li>Be located in close proximity to town centres and community hubs;</li> <li>Be accessible by public transport;</li> <li>Not present a barrier to movement through the surrounding road network; and</li> <li>Be located outside the pipeline measurement length where possible.</li> </ul>

**Table 2** Housing type by lot size

The following table provides an example of the typical housing types that might be provided on a range of lot sizes that support the housing diversity objectives.

HOUSING TYPES THAT MAY BE SUPPORTED	LOT SIZE CATEGORY (M <sup>2</sup> )		
	LESS THAN 300	301 - 600	MORE THAN 600
Small lot housing (including town houses and attached, semi-detached and detached houses)			
Dual occupancies, including duplex			
Detached housing			
Multi-unit housing sites (including terraces, row houses and villas)			
Stacked housing (including apartments and walk-up flats)			

**Table 3** Housing delivery guide

RESIDENTIAL TYPE	NDA (HA)	DWELLINGS /NDAHA	TOTAL DWELLINGS
Residential within walkable catchment and residential outside walkable catchment	581.21		10,317
Residential within walkable catchment	161.49	21.0	3,391
Residential outside walkable catchment	419.72	16.5	6,925
Town Centres or Mixed Use	17.70	25	443
<b>Totals</b>	<b>598.91</b>		<b>10,759</b>

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## 3.2 Employment and Town Centres

### 3.2.1 Major Town Centre

The Major Town Centre (MTC) is located at the cross roads between the historic Beattys Road Reserve and new extensions to two arterial roads, forming a unique place-making structure for the centre. The urban form is delineated by the new and historic road structure, which is partly transformed into open space to create a strong framework for the centre which has at its heart key community gathering places. Additionally, a major waterway element has the potential to form a more naturalistic backdrop and break in the urban form as a linear wetland/ retarding basin linking to the broader open space network.

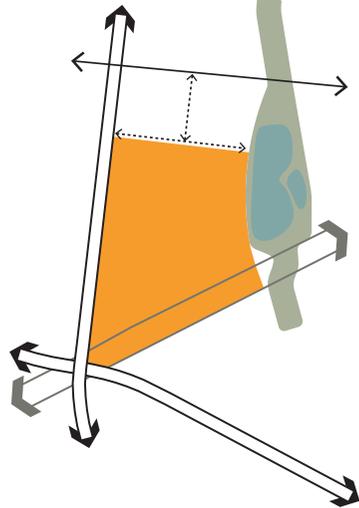
The MTC will form the heart of a regional catchment and will provide retail, commercial, cultural, community, sporting and social facilities. Adjacency to the Business and Industrial Precinct to the west will further strengthen the diverse employment potential and offer of the centre.

**Figure 5 Major Town Centre Organising Elements**

The 'organising elements' diagrams are not intended to be prescriptive. They illustrate the key drivers behind the development of the MTC concept plan.

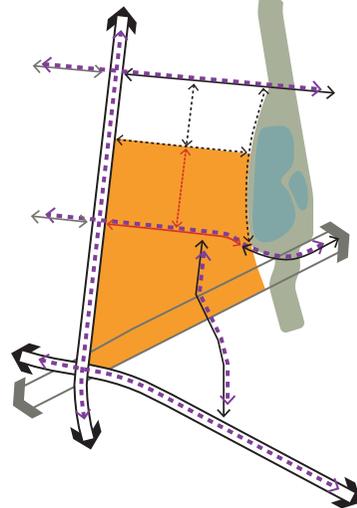
**Figure 5a**

INDICATIVE URBAN DESIGN FRAMEWORK EXTENT



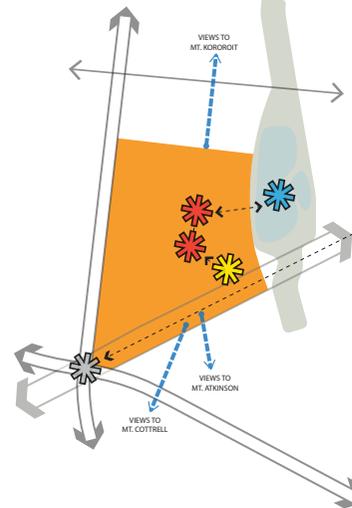
**Figure 5b**

INDICATIVE ROAD & TRANSPORT NETWORK



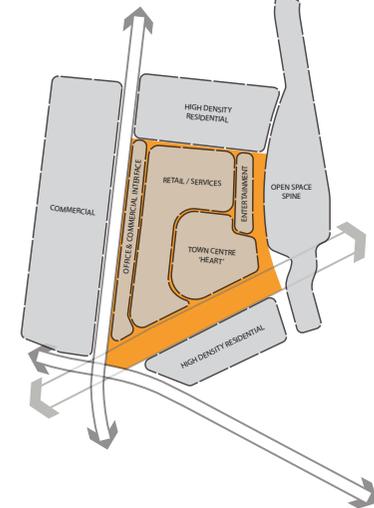
**Figure 5c**

INDICATIVE PLACEMAKING



**Figure 5d**

INDICATIVE PRECINCTS



- The MTC is shaped by the future Hopkins Road extension (north-south); the existing Beattys Road Reserve; the retarding basin and waterway corridor; and the future extension of Tarletons Road (west – southeast); and
- These form the major design constraints and opportunities for the MTC, as well as the extent of the Urban Design Framework to be prepared.

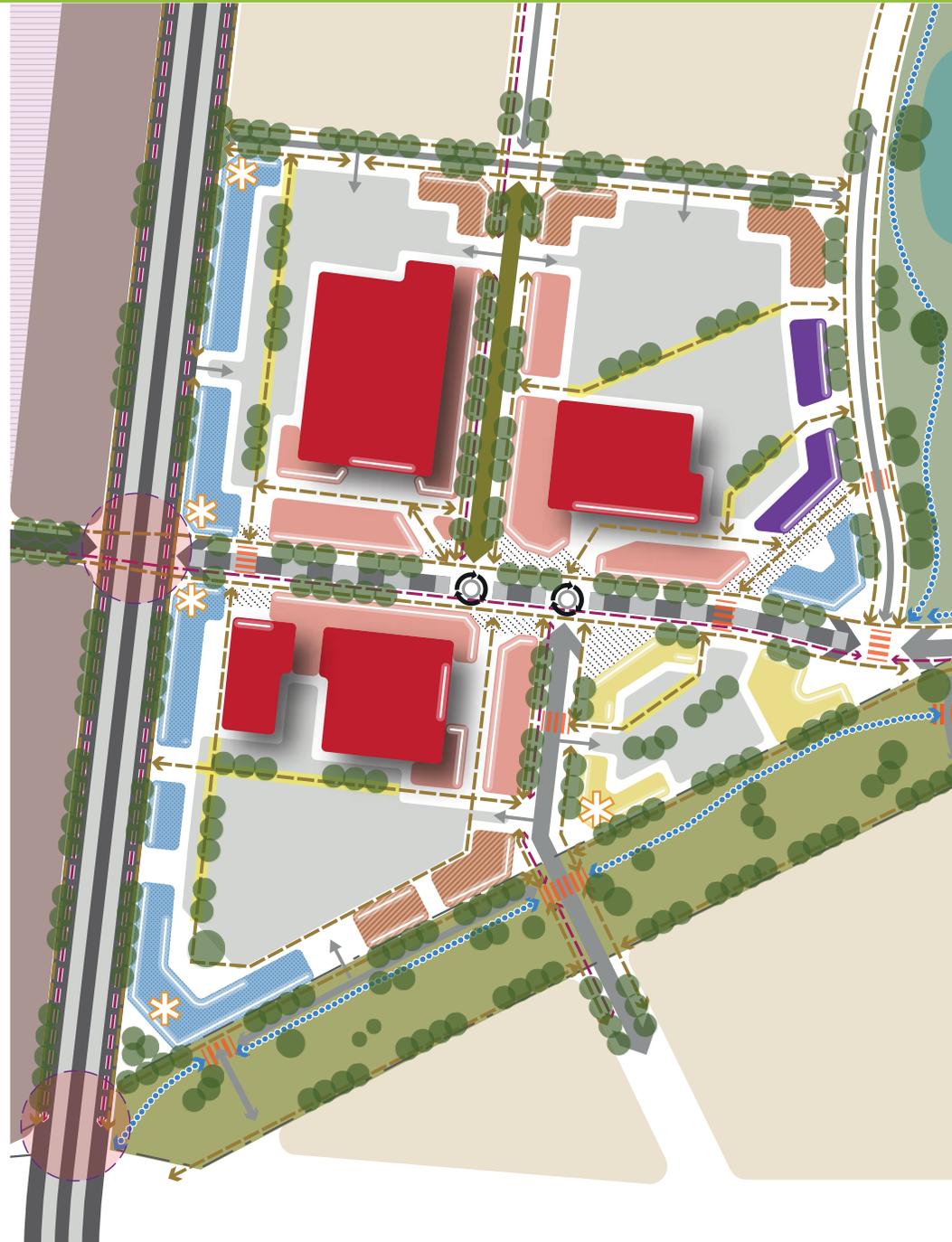
- Beattys Road Reserve, and the north-south waterway will provide pedestrian and cycling access into the MTC;
- Connector streets and Hopkins Road will provide access for all modes; and
- The east-west main street is the focus for civic facilities, retail and the town square, with supporting north-south streets.

- The six-way 'pinwheel' intersection area is an important gateway site, which shapes the structure of the MTC;
- Beattys Road historic goldfields route will be developed into a tree-lined linear reserve with streets along one or two sides along its length throughout, and punctuated with community facilities;
- The waterway and retarding basin will be attractively landscaped to provide a green space and pleasant outlook for the MTC; and
- A new town square will focus activity within the MTC.

- The town centre 'heart' includes the town square, civic facilities and the retail core;
- Further anchor and specialty retail shops are located in the retail/ service precinct;
- The entertainment precinct will include cafes and restaurants, taking advantage of the attractive outlook over the open space spine formed by the waterway to the east ;
- An office and commercial area will be the 'face' of the MTC to Hopkins Road, and be well connected to further commercial opportunities west of Hopkins Road; and
- Higher density residential areas north and south of the MTC will contribute to the vibrancy of the MTC, together with opportunities for higher density residential development over other retail/ commercial uses in the MTC and along Hopkins Road commercial uses to the west.

- |  |                                |  |
|--|--------------------------------|--|
| area subject to urban design framework | connector street - town centre | town square                              |
| Beattys Road reserve                   | feature main street            | waterway, wetlands & retarding basin     |
| arterial road                          | local access street            | Beattys Road historical goldfields route |
| connector street                       | bus capable street             | 6-way 'pinwheel' street structure        |
| connector street - industrial          | drainage                       | community node                           |

- anchor retail
- retail (other potential uses on upper storeys)
- mix of commercial uses (other potential uses on upper storeys)
- mixed use (must have business / other employment use on ground floor at minimum) with loop road frontage
- entertainment / food
- commercial (other potential uses on upper storeys)
- business
- car parking
- community facilities
- residential
- active frontage
- accentuated height
- high quality public realm
- pedestrian priority (e.g. pavement treatment)
- retarding basin / waterway
- beattys road linear reserve
- extent of beattys road reserve
- arterial road (6 lanes)
- connector street
- connector street - town centre
- connector street - industrial
- feature main street
- local access street
- roundabout (including pedestrian / cycle crossing points)
- signalised intersection
- priority crossing (e.g. raised path)
- bike route
- shared path
- key pedestrian circulation
- car park access point



**PLACE-MAKING AND DESIGN ELEMENTS**

- The historic Beattys Road Reserve meets with the new street grid of the PSP to shape the structure of the Plumpton Major Town Centre.
- Beattys Road Reserve is partially transformed into a linear reserve with multiple functions along its length including street, shared path and parkland, encouraging walking and cycling into the MTC.
- Major supermarkets and a discount department store are retail anchors, and specialty retail and a town square along two intersecting “main” streets accentuate the importance of this location as the heart of the area.
- Diverse employment uses within the MTC are strengthened by links to business and industrial land across Hopkins Road.
- The east-west main street provides strong links to civic uses to the east, and connects to the Beattys Road Reserve and the street network.
- The waterway is an open space asset encouraging outdoor dining and related uses adjacent.
- Locating a high quality building of increased scale and height at the Hopkins Road/ Beattys Road Reserve intersection emphasises the unusual ‘pinwheel’ street structure, defines the MTC’s edge and provides a local landmark.

*The MTC concept plan is a concept plan and is not intended to be prescriptive.*

REQUIREMENTS	
<b>R17</b>	Shop floor space within the Major Town Centre (excluding 'restricted retail') must not exceed 45,000m <sup>2</sup> without a planning permit.
<b>R18</b>	An Urban Design Framework Plan (UDF) must be approved by the responsible authority for the Major Town Centre within the UDF area as illustrated on Figure 6.  The UDF must address the following: <ul style="list-style-type: none"> <li>• A response to Figure 5, Figure 6 and Appendix B;</li> <li>• Relevant design guidelines prepared by the Victorian Government and Melton City Council;</li> <li>• A land use plan that identifies the appropriate location for all supported land uses and considers the relationship between these uses, including the integration of community facilities and services;</li> <li>• A fine-grain street network that identifies direct connectivity within, to and from the town centre, including measures to slow down speeds along main streets;</li> <li>• Measures to prioritise pedestrians along and across the main streets, and provide a continuous path of travel for pedestrians and cyclists to key destinations;</li> <li>• Locations of public transport services, including bus stops;</li> <li>• A diversity of sizes and types of commercial tenancies;</li> <li>• Higher density housing within and surrounding the town centre, and its design;</li> </ul>
<b>R19</b>	<ul style="list-style-type: none"> <li>• Staging and indicative development timing of the town centre;</li> <li>• Provisions for car parking including the location and design of parking areas, car parking rates and a demonstration of how off-street car parking has been minimised through efficiencies in the shared use of off-street facilities;</li> <li>• Provision of service areas for deliveries and waste disposal including access for larger vehicles, including measures to minimise negative impacts on the amenity of the town centre and adjoining neighbourhoods;</li> <li>• Design of the interface with the Plumpton Business and Industrial Precinct; Beattys Road Reserve; the Hopkins Road/ Tarletons Road/ Beattys Road 'pinwheel' intersection; the waterway corridor/drainage infrastructure; and surrounding residential uses;</li> <li>• Key views to the surrounding area and open space and the creation of vistas through the town centre to create interest in the streetscape and provide opportunities for fine grained urban design outcomes;</li> <li>• A public space plan that identifies a hierarchy of public spaces including local parks, pedestrian and cycling links, urban spaces and landscape nodes, showing links to the broader open space network;</li> </ul>

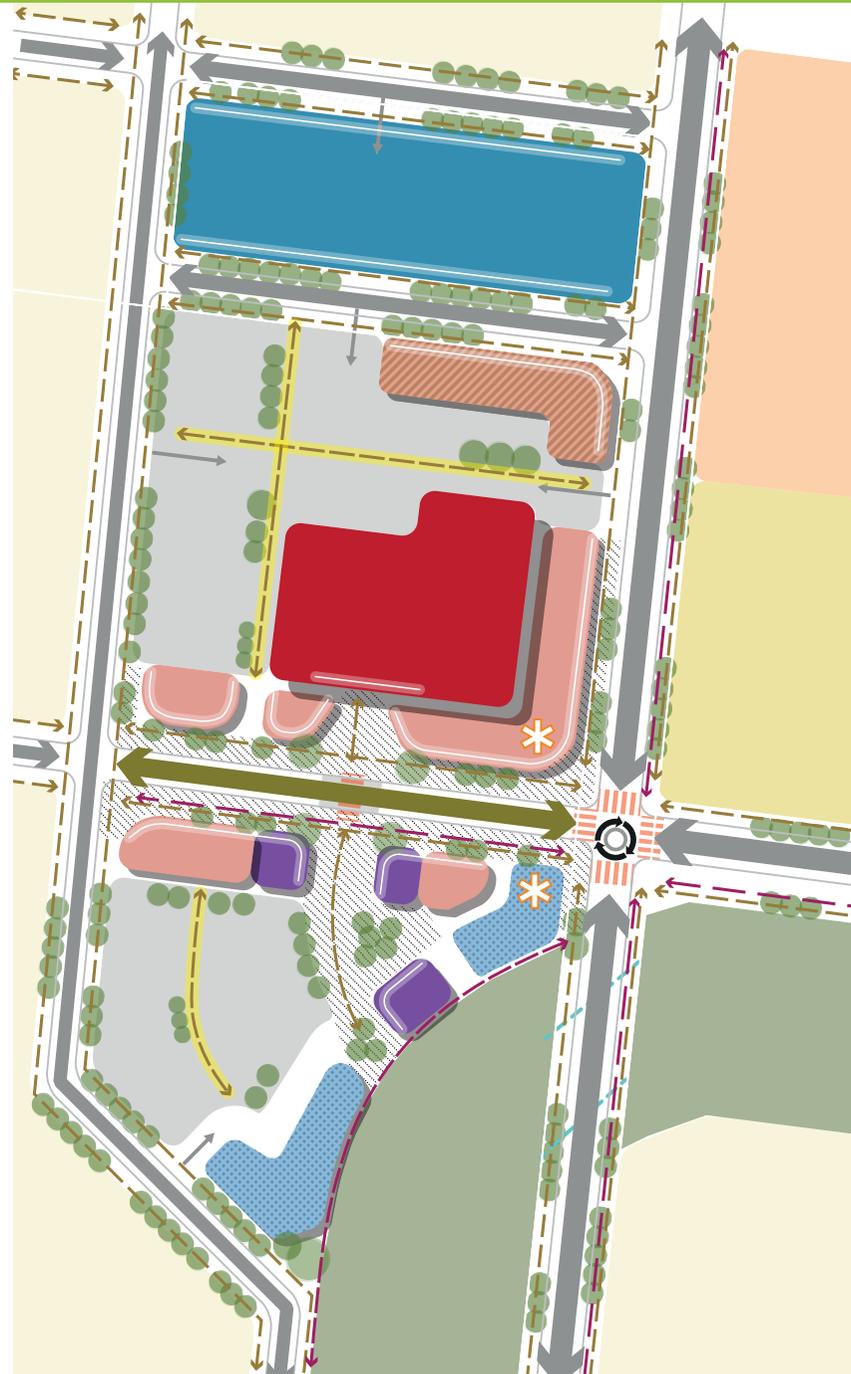
<b>R19</b> <i>cont'd...</i>	<ul style="list-style-type: none"> <li>• Location and design of active uses, signage and treatment of ground floor windows (ie frosting and advertising should not cover windows, to ensure views in and out of ground floor tenancies are maintained);</li> <li>• Visual interest at the pedestrian scale with active and activated façade treatments, avoiding long expanses of unarticulated façade treatments;</li> <li>• Place-making elements, character precincts and destinations within the town centre including a hierarchy of public spaces that provide opportunities for social interaction and local events; and</li> <li>• Requirements for a variety of building materials and form.</li> </ul> <p>The UDF must set out clear and specific guidelines for the development of the centre, responding to the above, and will be used as an assessment tool for future development applications within the centre unless otherwise agreed by the responsible authority.</p>
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**Table 4** Anticipated employment creation in precinct

LAND USE	EMPLOYMENT MEASURE	JOBS PER EMPLOYMENT MEASURE	ANTICIPATED QUANTITY OF EACH LAND USE	ANTICIPATED QUANTITY OF JOBS
Community centre	Jobs per centre	10	3	30
Primary School	Jobs per school	40	4	160
Secondary School	Jobs per school	90	2	180
Other community services (medical, NGO etc)	Jobs per 50 m <sup>2</sup> floor space	0.02	21,240	425
Retail	Jobs per 30 m <sup>2</sup> floor space	0.033	84,960	2,804
Commercial/ mixed use	Jobs per 20 m <sup>2</sup> floor space	0.05	42,400	2,120
Industrial/ commercial employment area	Jobs per 60 m <sup>2</sup> floor space	0.017	382,200	6,497
Home based business	Jobs per 20 dwellings	0.05	10,759	538
<b>Total estimated</b>				<b>12,754</b>

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- small local enterprise
- anchor retail
- retail (commercial / mixed use / residential over)
- mix of commercial uses (residential over)
- commercial (residential over)
- entertainment / food
- car parking
- community facilities
- government primary school
- residential
- active frontage
- accentuated height
- constructed waterway (indicative)
- drainage culvert (indicative)
- high quality public realm treatment
- pedestrian priority (e.g. pavement treatment)
- connector street
- feature main street
- local access street
- roundabout (including pedestrian / cycle crossing points)
- pedestrian priority crossing
- key pedestrian circulation
- bike route
- car park access



**PLACE-MAKING AND DESIGN ELEMENTS**

- Activity is focused along the central, pedestrian-priority main street.
- Specialty retail shops provide an active and attractive interface to the street, 'sleeved' around a supermarket anchor.
- A high quality connection across the connector road provides strong links to community and education uses to the east.
- Built form along the edges of the LTC provide an attractive interface to surrounding residential uses.
- The waterway shapes the LTC and provides an attractive outlook for cafes/ entertainment and a public plaza, as well as office workers.
- Opportunities for height along Plumpton Road draw attention to the LTC within the surrounding residential area.
- The adjacent constructed waterway/swale links to the broader open space network and will be an attractive part of the landscape.
- Small local enterprises located on the periphery of the LTC provide for diverse local businesses and services.

*The LTC concept plan is a concept plan and is not intended to be prescriptive.*

### 3.2.2 Local Town Centre

Plumpton Local Town Centre will service local needs and will provide opportunities for ‘small local enterprises’ to develop and form part of the LTC and community hub. It is located close to a future sports reserve and adjacent to community facilities and a government and non-government school, and will enable a strong sense of place through responding to an attractive landscaped waterway swale and the existing Plumpton Road.

‘Small local enterprises’ are supporting services and ancillary uses which are typically on the periphery of, or near, Local Town Centres in traditional inner and middle ring areas in Melbourne. Uses may include, but are not limited to the following (subject to planning permit requirements):

Printers, craft centres, storage, equipment repairs and servicing, studio/ workrooms, veterinary clinics, dance studios.

They can require many different layout options, varied floor space sizes, servicing, storage and lower-order rentals than in the core retail areas. These are integral to the creation of LTCs and help support three of the principles in Appendix C, by:

- Providing a full range of local services (Principle 4 - adapted)
- Integrating local employment and service opportunities (Principle 6 - adapted)
- Promoting sustainability, adaptability and localisation (Principle 10 - adapted).

REQUIREMENTS	
<b>R20</b>	Land use and development within the Local Town Centre must respond to the concept plan in Figure 7 and address Appendix C, unless otherwise approved by the responsible authority.
<b>R21</b>	Design of buildings in the Local Town Centre must provide visual interest at the pedestrian scale, with active and articulated façade treatments. Long expanses of unarticulated façade treatments must be avoided.
<b>R22</b>	Shop floor space within the Local Town Centre must not exceed 7,200 square metres without a planning permit.

### 3.2.3 Local Convenience Centres

The Local Convenience Centres will service local needs and will provide opportunities for some small local enterprises to develop. The LCC in the south-east of the precinct will develop into a community hub as it is co-located with future sporting reserves, primary school and community facilities.

REQUIREMENTS	
<b>R23</b>	Local Convenience Centres must be oriented towards the connector street and consider the relationship and interface with surrounding uses.
<b>R24</b>	Shop floor space within the local convenience centre in the Plumpton Local Convenience hub must not exceed 1000m <sup>2</sup> without a planning permit.
<b>R25</b>	Buildings as part of a local convenience centre must: <ul style="list-style-type: none"> <li>• Provide primary access to tenancies from the connector street;</li> <li>• Provide active and articulated frontages to the adjoining street network;</li> <li>• Have active frontages and must be designed in a way which contributes to the public domain; and</li> <li>• Locate any servicing infrastructure or car parking to the rear or centre of the allotment in a manner that protects the amenity of the surrounding neighbourhood.</li> </ul>
<b>R26</b>	Safe and convenient pedestrian access must be provided to the local convenience centre, including a safe pedestrian street crossing and proximity to bus stop locations.
GUIDELINES	
<b>G21</b>	A local convenience centre should be located as illustrated on Plan 3 and on Figure 8, unless otherwise agreed by the responsible authority.
<b>G22</b>	The design of the Local Convenience Centre in the south-east of the precinct should: <ul style="list-style-type: none"> <li>• Feature clear circulation and a high degree of permeability for pedestrians;</li> <li>• Provide for a mix of tenancies; and</li> <li>• Incorporate a range of uses including retail, offices and medium density residential use.</li> </ul>

- precinct boundary
- current property boundary
- industrial
- business
- mixed use: business / employment / retail use on ground floor at minimum, with opportunity for residential or other uses on upper storeys.
- major town centre
- residential
- ovr public acquisition overlay
- substation (indicative only)
- waterway
- local park
- landscaping & setback requirement at interface
- attractive frontage to waterway / arterial road
- beattys road linear reserve
- road bridge
- arterial road (6 lanes)
- arterial road (4 lanes)
- connector street
- connector street - town centre
- connector street - industrial
- industrial access street
- key local access street
- key local access street - industrial (indicative only)
- signalised intersection
- shared path
- local convenience centre

**NOTE:**  
 To provide convenient goods and services to workers in the business and industrial precinct, shop floor space (excluding 'restricted retail' uses) can be up to 1,000 sq metres and commercial floor space up to 1,000 sq. metres in local convenience centre.



### 3.2.4 Business and industrial precinct

The Business and Industrial Precinct will service the emerging residential communities in Plumpton and areas to the south and west. Its strategic location between the Plumpton Major Town Centre/ Hopkins Road, Melton Highway and the future Outer Metro Ring will make it a highly accessible location for a range of businesses, as well as a highly accessible location in which to work.

The focus will be on local and sub-regional businesses which can benefit from the location close to emerging residential communities and provide services to these communities – such as storage, printing, automotive and equipment repairs and supplies; ‘factoryettes’; distribution and postal services; workshops and studios for emerging local businesses; as well as other uses which need larger floor plates than typically available in town centres.

With a variety of lot sizes available, the Business and Industrial Precinct will support a diversity of businesses. An attractive waterway, local parks and a local convenience centre will provide workers with places to lunch, play informal sport and walk along, as well as providing connections to neighbouring non-urban and future PSP areas.

Smaller lots with higher density job figures will be encouraged in areas closer to the Major Town Centre and public transport services, and are also likely on the more sloping topography adjacent the waterway.

Medium-density live-work options will be encouraged along the western side of Hopkins Road to facilitate a diversity of employment and development options and provide an active interface to the MTC. Housing must only occur on upper floors, with employment uses on the ground floor. Frontage to adjacent existing and proposed arterial roads will provide a high degree of exposure for new businesses, facilitating early development. The interface of the Business and Industrial Precinct to residential areas in particular will require well-designed buildings with landscaped setbacks.

The following requirements apply to the Business and Industrial Precinct on Plan 5.

REQUIREMENTS	
<b>R27</b>	Development proposals in the Business and Industrial Precinct must respond to Figure 8 and relevant industrial guidelines, as well as relevant design for safety guidelines unless otherwise approved by the responsible authority
<b>R28</b>	Location of land uses, building design, and interface treatments in industrial, mixed use and business areas shown on Plan 3 must minimise negative impacts on the amenity of nearby residential areas.

<b>R29</b>	A shared path must be provided on both sides of the waterway, and a street must be provided along at least one side of the waterway. This enables greater flexibility of lot sizes and ensures the developability and feasibility of development in the Business and Industrial Precinct, while ensuring pedestrian and cyclist connectivity is maintained and that buildings present an attractive address to the waterway. Where a street is not proposed along a waterway, buildings must front this waterway and associated shared path.
<b>R30</b>	Goods and materials storage areas and refuse areas must not be visible from public areas.
<b>R31</b>	Buildings and car parking or other areas along Hopkins Road and/ or Tarletons Road in the Business and Industrial Precinct must be set back a minimum of 5 metres and landscaped to provide an attractive interface to surrounding areas.
<b>R32</b>	Key locations including arterial and connector/ arterial intersections; and adjacent to local parks must incorporate features of interest into the built form and surrounding landscape, including: <ul style="list-style-type: none"> <li>• Variations in built form elements (such as building heights, use of parapets, awnings, shade structures, balconies, and roof elements);</li> <li>• Articulation of building facades; and</li> <li>• Feature colours and materials.</li> </ul>
<b>R33</b>	Vehicular access to properties fronting Tarletons Road and Hopkins Road must be from service roads, internal loop roads and/or rear laneways. Service roads and internal loop roads must provide indented parking lanes to cater for on street parking.
<b>R34</b>	An Urban Design Framework Plan (UDF) must be approved by the responsible authority for the part of the Business and Industrial Precinct indicated in Plan 5.
<b>R35</b>	The UDF for the Business and Industrial Precinct must address the following: <ul style="list-style-type: none"> <li>• A response to Figure 8;</li> <li>• Relevant Melton City Council design guidelines;</li> <li>• A land use plan that identifies the appropriate location for all supported land uses and considers the relationship between these uses;</li> <li>• Ground floor uses along Hopkins Road (must be ‘employment’ uses eg offices, business, studios, workshops, retail);</li> <li>• Locations for medium and higher density housing – permitted along Hopkins Road only, and only on upper floors, and only where there are ‘employment’ uses on the ground floor as reflected on Figure 8;</li> <li>• Entries to Hopkins Road housing and employment uses;</li> <li>• The interface between mixed use and business areas;</li> <li>• A street network that identifies direct connectivity to and from the Major Town Centre, including measures to slow down speeds along main streets and truck turning movements;</li> </ul>

<p><b>R35</b> <i>cont'd...</i></p>	<ul style="list-style-type: none"> <li>• Measures to prioritise pedestrians along and across the main streets, and provide a continuous path of travel for pedestrians and cyclists to key destinations;</li> <li>• Locations of public transport services, including bus stops;</li> <li>• A diversity of sizes and types of commercial tenancies;</li> <li>• Staging and indicative development timing;</li> <li>• Provisions for car parking including the location and design of parking areas, car parking rates and a demonstration of how off-street car parking has been minimised through efficiencies in the shared use of off-street facilities;</li> <li>• Provision of service areas for deliveries and waste disposal including access for larger vehicles, which includes measures to minimise negative impacts on the amenity of the area;</li> <li>• Design of the interface with the Plumpton Major Town Centre; Beattys Road Reserve; the Hopkins Road/ Tarletons Road/ Beattys Road 'pinwheel' intersection; and surrounding residential and industrial/ business uses;</li> <li>• Location and design of active uses, signage and treatment of ground floor windows (ie frosting and advertising should not cover windows, to ensure views in and out of ground floor tenancies are maintained);</li> <li>• Visual interest at the pedestrian scale with active and activated façade treatments, avoiding long expanses of unarticulated façade treatments; and</li> <li>• Requirements for a variety of building materials and form.</li> </ul> <p>The UDF must set out clear and specific guidelines for the development of the area, responding to the above, and will be used as an assessment tool for future development applications within the area, unless otherwise approved by the responsible authority.</p>
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**GUIDELINES**

<b>G23</b>	Subdivision should provide for the creation of a range of lots sizes to cater for a diversity of commercial uses.
<b>G24</b>	Ancillary offices should be located at the front of buildings; should include a façade addressing the street frontage of the lot; and provide for improved pedestrian access and engagement with the public domain.
<b>G25</b>	Any visitor car parking and access areas in the front setback area should be setback a minimum of 3 metres from the street frontage to enable provision of sufficient landscape strips at the street frontage. All vehicles should be able to enter/exit the site in a forward direction.
<b>G26</b>	Large expanses of continuous wall visible to the street should have appropriate articulation, landscaping and other elements to provide relief and visual interest.

<b>G27</b>	A consistent landscaping theme should be developed along streets and access ways. Variations in street tree species should be used to create visual cues in appropriate locations such as at the termination of view lines, key intersections, and in parks.
<b>G28</b>	Streets should be aligned to create views and direct connections to local parks and waterways.
<b>G29</b>	Water tanks, service infrastructure and other structures (including plant and equipment) that are not part of the building should be located behind the building line or where this is not possible, behind constructed screening using durable and attractive materials.
<b>G30</b>	Buildings to be located at the front of any site and car parking and loading facilities to be located to the side or rear of any buildings to present an attractive address to the street.

### 3.3 Community facilities and education

#### 3.3.1 Community facilities and education

REQUIREMENTS	
<b>R36</b>	<p>Where the responsible authority is satisfied that land shown as a non-government school site is unlikely to be used for a non-government school, that land may be used for an alternative purpose which is generally consistent with the surrounding land uses and the provisions of the applied zone. In order to satisfy the responsible authority that a site is unlikely to be used for a non-government school, it is necessary to demonstrate that:</p> <ul style="list-style-type: none"> <li>• The application for an alternative use is not premature having regard to the extent of development in the surrounding residential area</li> <li>• The school site is no longer strategically justified having regard to the provision of schools in the locality, including land not within the PSP, as appropriate</li> <li>• The landowner provides the responsible authority with evidence that: <ul style="list-style-type: none"> <li>• Genuine negotiations have been had with a range of education providers including the lead agency nominated in the PSP, regarding the use of the site as a school and the sale of site to the education provider/s; and</li> <li>• The educational provider(s), including the lead agency nominated in the PSP, do not intend to Provision and use the site as a school.</li> </ul> </li> </ul>
<b>R37</b>	Connector or local access streets abutting a school must be designed to achieve slow vehicle speeds and provide designated pedestrian crossing points as required by the responsible authority.
GUIDELINES	
<b>G31</b>	The design and layout of schools, community facilities and sports reserves should be integrated where possible with neighbouring facilities, and fencing minimised, to enable community use of facilities out of hours; to deliver continuous pedestrian paths of travel; and to achieve efficiencies such as sharing and overall reduction of car parking spaces.
<b>G32</b>	The design and layout of schools, community facilities and sports reserves should include canopy tree planting to provide appropriate shade.
<b>G33</b>	Community facilities should be planned and designed to have the flexibility and capacity to meet the changing needs of the community and provide for a range of community uses.
<b>G34</b>	Any private childcare, medical, or similar facility is encouraged to locate in or near Plumpton Major Town Centre, Local Town Centre, the Local Convenience Centre in the south-east of the precinct, and community hubs.

<b>G35</b>	The location of key entries to community facilities should allow for activation of the street and safe and convenient pedestrian and cyclist access for all ages and abilities.
<b>G36</b>	Schools should be provided with three street frontages where practical.
<b>G37</b>	Schools and community facilities should be designed to front and be directly accessed from a public street with car parking located away from the main entry.



- precinct boundary
- local sports reserve
- local park (within residential areas)
- local park (within dedicated employment areas)
- waterway reserve
- easement
- uncredited open space (beattys road reserve)
- open space & recreation identification number (refer to Table 7)
- local park & sports reserve residential catchment - 400 metres
- local sports reserve residential catchment - one kilometre
- pedestrian / cyclist crossing / bridge

### 3.4 Open space and biodiversity

#### 3.4.1 Open space

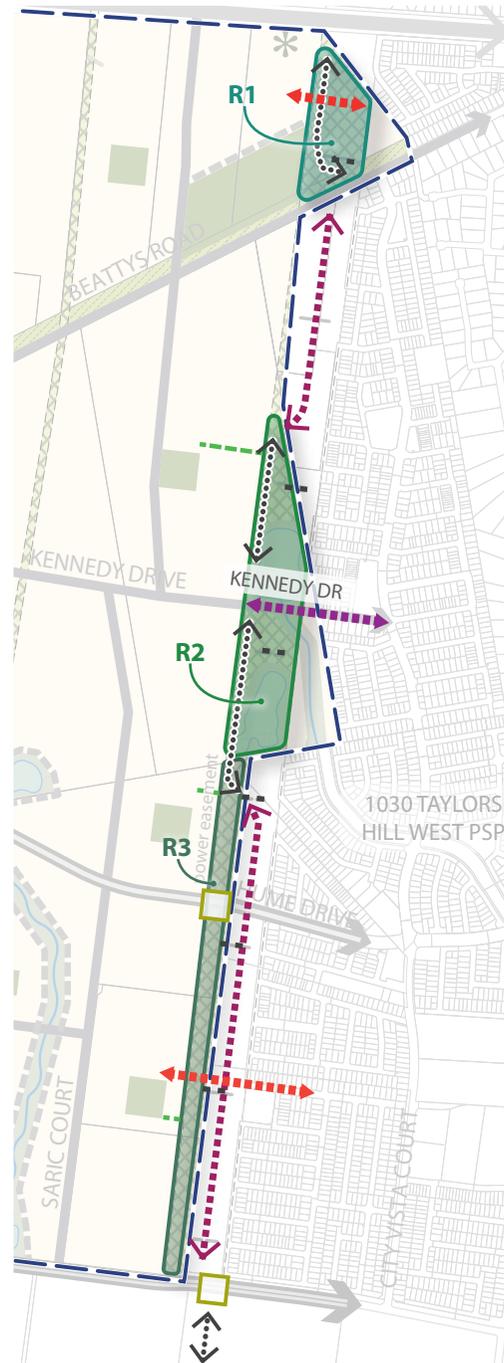
REQUIREMENTS	
<b>R38</b>	<p>All parks must be located, designed and developed to the satisfaction of the responsible authority generally in accordance with Plan 6 and Table 6 of this PSP. An alternative provision of land for a local park is considered to be generally in accordance, provided:</p> <ul style="list-style-type: none"> <li>• The location does not reduce the walkable access to local parks demonstrated on Plan 6;</li> <li>• The design does not diminish the quality or usability of the space for passive recreation;</li> <li>• The total provision of open space for a local park is equal to or more than the local park provision shown in Plan 6 and Table 6, or:</li> <li>• Where a proposed park is larger than outlined in the table it may be accepted so long as it does not result in the removal of another park allocation;</li> <li>• Where it is proposed to provide two or more local parks instead of a local park described in Table 6, the combined area of those parks must not be less than the area of the local park described in Table 6 and the responsible authority must be assured that this will be delivered</li> </ul> <p>Subject to the approval of the responsible authority, where a proposed park is smaller than outlined in Table 6, the land must be added to another park and the responsible authority must be assured that this will be delivered.</p>
<b>R39</b>	All open space and public landscaped areas must contain large-canopy trees appropriate to the local climate and soil conditions that are suitable to the urban environment, to the satisfaction of the responsible authority.
<b>R40</b>	Where fencing of open space is required, it must be low scale and/or visually permeable to facilitate public safety and surveillance.
<b>R41</b>	Where a local park shown on Plan 6 spans across multiple properties, the first development proponent to lodge a permit application must undertake a master plan for the entire park unless otherwise agreed by the responsible authority.
<b>R42</b>	Land designated for local parks must be finished and maintained to a suitable standard, prior to the transfer of land, to the satisfaction of the responsible authority.
<b>R43</b>	Design of service open space including waterway corridors, utilities easements and any other encumbered open space must maximise the amenity value of that open space and provide for flexible recreational opportunities, particularly when such land also abuts unencumbered open space.

<b>R44</b>	Appropriately scaled lighting must be installed along all major pedestrian thoroughfares traversing public open space and along the cycling network to the satisfaction of the responsible authority.
<b>R45</b>	Development of land in a subdivision which includes the powerlines easement must include landscaping for a width of at least 10 metres along both edges, to the satisfaction of the responsible authority and AusNet Services.
<b>R46</b>	Development of land in a subdivision which includes the high pressure gas transmission pipeline easement must include landscaping of the full easement width to the satisfaction of the responsible authority and APA.

GUIDELINES	
<b>G38</b>	Local parks should cater for a broad range of users by providing a mix of spaces and planting to support both structured and unstructured recreational activities and play opportunities for all ages and abilities, as well as supporting biodiversity.
<b>G39</b>	Any existing vegetation, including grassland, that can be viably maintained, should be protected and enhanced through open space networks which facilitate habitat and movement corridors for species found within the region of the precinct.
<b>G40</b>	Design of local parks and sporting reserves should demonstrate integration with the values of adjoining encumbered land including Aboriginal and post-contact heritage and drainage waterways (for example through adopting a similar planting palette, through minimising fencing or through landscape design).
<b>G41</b>	To enable good passive surveillance, open space should have an active frontage.
<b>G42</b>	Design for safety guidelines, and in particular the provision of active frontage and good passive surveillance from adjoining development, should guide the design of open spaces and associated infrastructure.
<b>G43</b>	Path networks associated with open space located inside and outside of the precinct should include way finding signage which clearly identifies key destinations.
<b>G44</b>	Land in the powerlines easement should be utilised for open space, recreation and other activities including those outlined in Table 5 - Powerline Easement Possible Use and Development and detailed on Figure 9 - Plumpton Powerline Easement Concept in accordance with <i>A Guide to Living with Transmission Line Easements</i> (AusNet Services) to the satisfaction of AusNet Services and the responsible authority.
<b>G45</b>	Where landscaping in the powerlines easement is required as part of subdivision, this should be provided to the satisfaction of AusNet Services, as follows: <ul style="list-style-type: none"> <li>• Provision of a shared path and planting of indigenous grasses and shrubs with full coverage over the area required to be landscaped</li> <li>• In accordance with Figure 9 – Plumpton Powerlines Easement Concept, Appendix H - Service Placement Guidelines and <i>A Guide to Living with Transmission Easements</i> (Ausnet Services).</li> </ul>
<b>G46</b>	Where landscaping in the gas easement is required as part of subdivision, this should be provided to the satisfaction of APA and the responsible authority, as follows: <ul style="list-style-type: none"> <li>• Planting of indigenous grasses and shrubs with full coverage over the area required to be landscaped</li> <li>• In accordance with Appendix F and APA guidance</li> <li>• Hard landscaping (e.g. street furniture) and small trees may be included provided sight lines between signs indicating the location of the pipe are not obscured and compliance with mandated pipeline clearances is achieved.</li> </ul>

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- precinct boundary
- shared path
- shared path  
(indicative - delivered as part of  
Taylors Hill West PSP)
- indicative links to open space  
(e.g. along local road)
- potential local street link
- connector street link
- pedestrian signals
- power transmission tower location
- R** recreation



**Table 5** Powerlines Easement Possible Use and Development

SUB-AREA (REFER TO FIGURE 9)	POSSIBLE USE AND DEVELOPMENT			
	PASSIVE RECREATION OPPORTUNITIES	ACTIVE RECREATION OPPORTUNITIES	TRANSPORT OPPORTUNITIES	UTILITIES/SERVICING OPPORTUNITIES
Recreation (R1)	<ul style="list-style-type: none"> <li>Market</li> <li>Community space</li> </ul>	Fitness circuit / other	<ul style="list-style-type: none"> <li>Local roads, 'park and ride' facility associated with future Melton Highway bus services.</li> <li>Bus stops are discouraged within the powerlines easement.</li> </ul>	<ul style="list-style-type: none"> <li>Potable water mains.</li> </ul>
Recreation (R2)	<ul style="list-style-type: none"> <li>Community gardens</li> <li>Dog off leash area</li> </ul>	Fitness circuit / other	<ul style="list-style-type: none"> <li>Local roads</li> </ul>	<ul style="list-style-type: none"> <li>Stormwater management</li> <li>Potable water mains</li> </ul>
Recreation (R3)	<ul style="list-style-type: none"> <li>Community gardens</li> <li>Dog off leash area</li> </ul>	Fitness circuit / other	<ul style="list-style-type: none"> <li>Local roads, 'park and ride' facility associated with future Tarletons Road bus services.</li> <li>Bus stops are discouraged within the powerlines easement.</li> </ul>	<ul style="list-style-type: none"> <li>Stormwater management</li> <li>Potable water mains</li> </ul>

Note: Subject to relevant approvals from the responsible authority and AusNet Services.

Amended by C203 **Table 6** Open space delivery guide

OPEN SPACE ID	LOCATION (PROPERTY ID)	TYPE	AREA (HECTARES)				SUB- CATEGORY/ SCALE	KEY ATTRIBUTES	RESPONSIBILITY FOR DELIVERY
			CREDITED OPEN SPACE	UNCREDITED OPEN SPACE	BEATTYS ROAD RESERVE	TOTAL			
LP-01	14	Local Park	0.50	0.00	0.00	0.50	Neighbourhood Park	Located within the industrial/business precinct abutting the waterway. Its location is associated with cultural values. Its design should respond to the area's local heritage and cultural values through appropriate interpretation.	Melton City Council & developer works
LP-02	12-E	Local Park	0.50	0.00	0.00	0.50	Neighbourhood Park	Located within the industrial/business precinct abutting the waterway. Its location is associated with high geomorphic and ecological values.	Melton City Council & developer works
LP-03	5	Local Park	0.50	0.00	0.00	0.50	Neighbourhood Park	Located within the industrial/business precinct abutting the waterway. Its location is associated with cultural values. Its design should respond to the area's local heritage and cultural values through appropriate interpretation.	Melton City Council & developer works
LP-04	10	Local Park	0.50	0.00	0.00	0.50	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-05	12-R	Local Park	0.25	0.00	0.00	0.25	Town Square	Town square centrally located within the Plumpton Major Town Centre, centred on the east-west feature main street near civic facilities and retail anchors. There is the opportunity for public art to be incorporated as part of its design. Amenity node for higher density housing.	Melton City Council & developer works

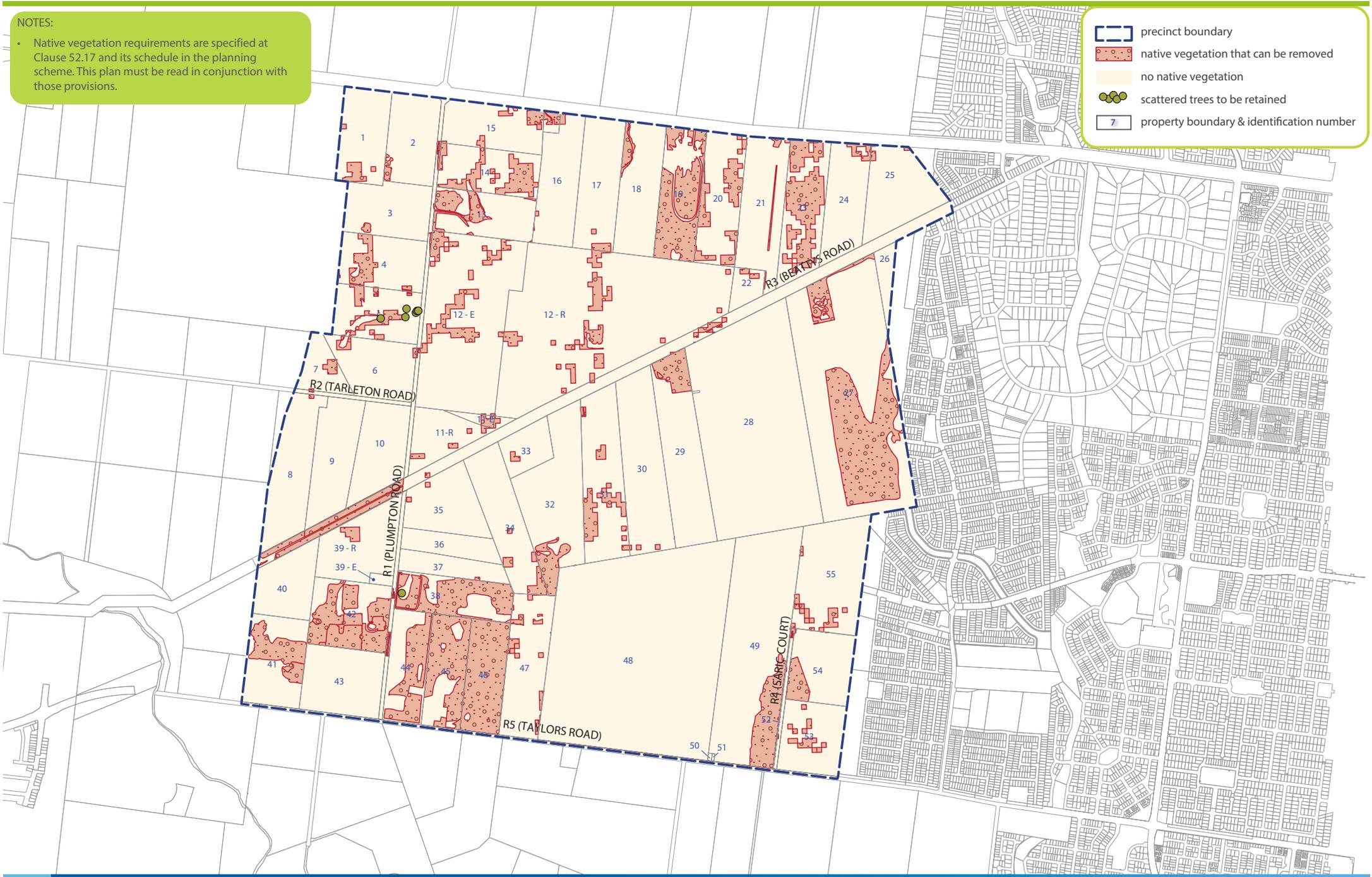
OPEN SPACE ID	LOCATION (PROPERTY ID)	TYPE	AREA (HECTARES)				SUB-CATEGORY/ SCALE	KEY ATTRIBUTES	RESPONSIBILITY FOR DELIVERY
			CREDITED OPEN SPACE	UNCREDITED OPEN SPACE	BEATTYS ROAD RESERVE	TOTAL			
LP-06	16	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-07	20	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-08	23	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-09	34	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-10	32	Local Park	0.80	0.00	0.00	0.80	Neighbourhood Park	Located adjacent to waterway. Includes a pedestrian bridge across the waterway to increase its accessibility.	Melton City Council & developer works
LP-11	28	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-12	27	Local Park	1.00	0.00	0.00	1.00	Community Park	Stand alone park.	Melton City Council & developer works
LP-13	55	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-14	49	Local Park	0.50	0.00	0.00	0.50	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-15	54	Local Park	1.00	0.00	0.00	1.00	Community Park	Stand alone park.	Melton City Council & developer works
LP-16	48	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park. Its location is associated with Aboriginal cultural values. Its design should respond to the area's local heritage and cultural values through appropriate interpretation.	Melton City Council & developer works
LP-17	47	Local Park	0.95	0.00	0.00	0.95	Community Park	Located adjacent to waterway. Includes a pedestrian bridge across the waterway to increase its accessibility.	Melton City Council & developer works
LP-18	45	Local Park	0.50	0.00	0.00	0.50	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LP-19	42	Local Park	1.00	0.00	0.00	1.00	Neighbourhood Park	Stand alone park.	Melton City Council & developer works
LOS-01	25, 26, 27, 53, 54, 55	Linear Open Space	0.00	18.43	0.67	19.10	District Linear Park	Power easement - including shared path, landscaping and embellishments.	Melton City Council & developer works

OPEN SPACE ID	LOCATION (PROPERTY ID)	TYPE	AREA (HECTARES)				SUB-CATEGORY/ SCALE	KEY ATTRIBUTES	RESPONSIBILITY FOR DELIVERY
			CREDITED OPEN SPACE	UNCREDITED OPEN SPACE	BEATTYS ROAD RESERVE	TOTAL			
LOS-02	23, 28, 49	Linear Open Space	0.00	6.32	0.15	6.47	District Linear Park	Gas easement - including shared path, landscaping and embellishments.	Melton City Council & developer works
LOS-03	R3	Linear Open Space	0.00	0.00	19.47	19.47	District Linear Park	Beattys Road Reserve - including shared path, landscaping and embellishments (including partial local road functions).	Melton City Council & developer works
SR-01	17, 18	Sports Reserve	6.50	0.00	0.00	6.50	District Park	Referred to as the <b>Plumpton North Sports Reserve</b> comprising sports reserve, pavilion, multipurpose hard courts (6 court facility) and associated infrastructure located adjacent to the waterway and future non-government secondary school.	Melton City Council
SR-02	24, 25, R3	Sports Reserve	5.29	0.00	0.71	6.00	District Park	Referred to as the <b>Plumpton East Sports Reserve</b> comprising sports reserve, pavilion and associated infrastructure located adjacent to the powerline easement and partially within Beattys Road reserve.	Melton City Council
SR-03	28,29,30,31	Sports Reserve	10.00	0.00	0.00	10.00	District Park	Referred to as the <b>Plumpton Sports Reserve</b> comprising a sports reserve, pavilion and associated infrastructure located adjacent to the government P-12 school, community centre and north-south natural waterway.	Melton City Council
SR-04	35, 36, 37	Sports Reserve	10.00	0.00	0.00	10.00	District Park	Referred to as the <b>Plumpton West Sports Reserve</b> comprising sports reserve, pavilion and associated infrastructure located adjacent to the government and non-government primary schools and the Beattys Road reserve.	Melton City Council
SR-05	48	Sports Reserve	6.50	0.00	0.00	6.50	District Park	Referred to as the <b>Plumpton South Sports Reserve</b> comprising sports reserve, pavilion, multipurpose hard courts (8 court facility) and associated infrastructure located adjacent the government primary school and community centre.	Melton City Council
IR-01	12-E, R3	Indoor Recreation	3.60	0.00	0.43	4.03	District Park	Referred to as the <b>Plumpton Aquatics Centre</b> comprising an aquatics centre co-located with community centre and partially located within the Beattys Road Reserve.	Melton City Council

**NOTES:**

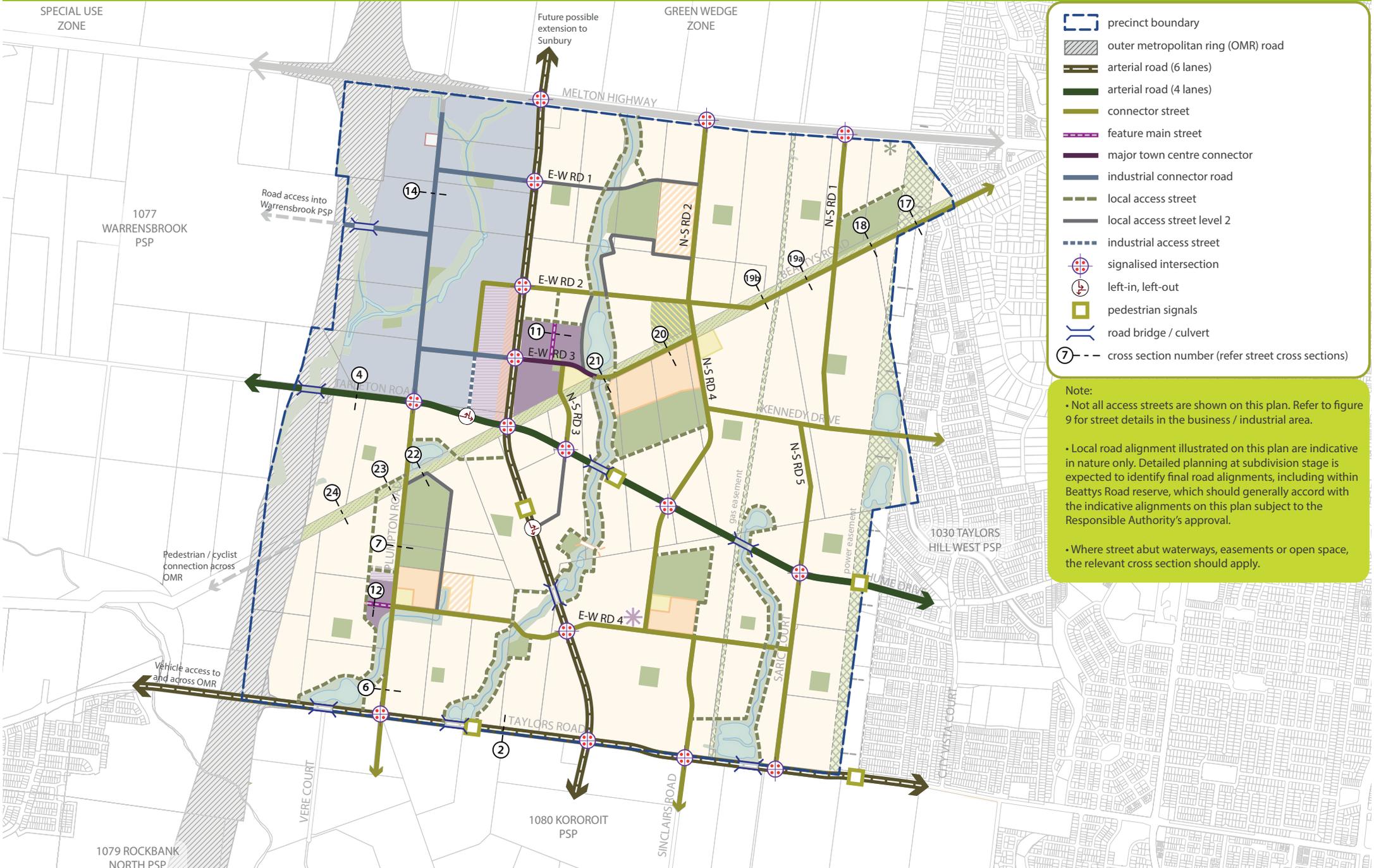
- Native vegetation requirements are specified at Clause 52.17 and its schedule in the planning scheme. This plan must be read in conjunction with those provisions.

- precinct boundary
- native vegetation that can be removed
- no native vegetation
- scattered trees to be retained
- property boundary & identification number



### 3.4.2 Biodiversity and threatened species

GUIDELINES	
<b>G47</b>	Planting in the open space network including streets, parks, utilities easements and waterways should make use of indigenous species to the satisfaction of the responsible authority (and Melbourne Water as relevant).
<b>G48</b>	The layout and design of the waterways, wetlands and retarding basins (including the design of paths, bridges and boardwalks and the stormwater drainage system) connecting to the Kororoit Creek to the south should integrate with biodiversity and natural systems to the satisfaction of the responsible authority, Melbourne Water, AusNet Services and the pipeline operator as relevant.



- precinct boundary
- outer metropolitan ring (OMR) road
- arterial road (6 lanes)
- arterial road (4 lanes)
- connector street
- feature main street
- major town centre connector
- industrial connector road
- local access street
- local access street level 2
- industrial access street
- signalised intersection
- left-in, left-out
- pedestrian signals
- road bridge / culvert
- cross section number (refer street cross sections)

**Note:**

- Not all access streets are shown on this plan. Refer to figure 9 for street details in the business / industrial area.
- Local road alignment illustrated on this plan are indicative in nature only. Detailed planning at subdivision stage is expected to identify final road alignments, including within Beattys Road reserve, which should generally accord with the indicative alignments on this plan subject to the Responsible Authority's approval.
- Where street abut waterways, easements or open space, the relevant cross section should apply.

### 3.5 Transport and movement

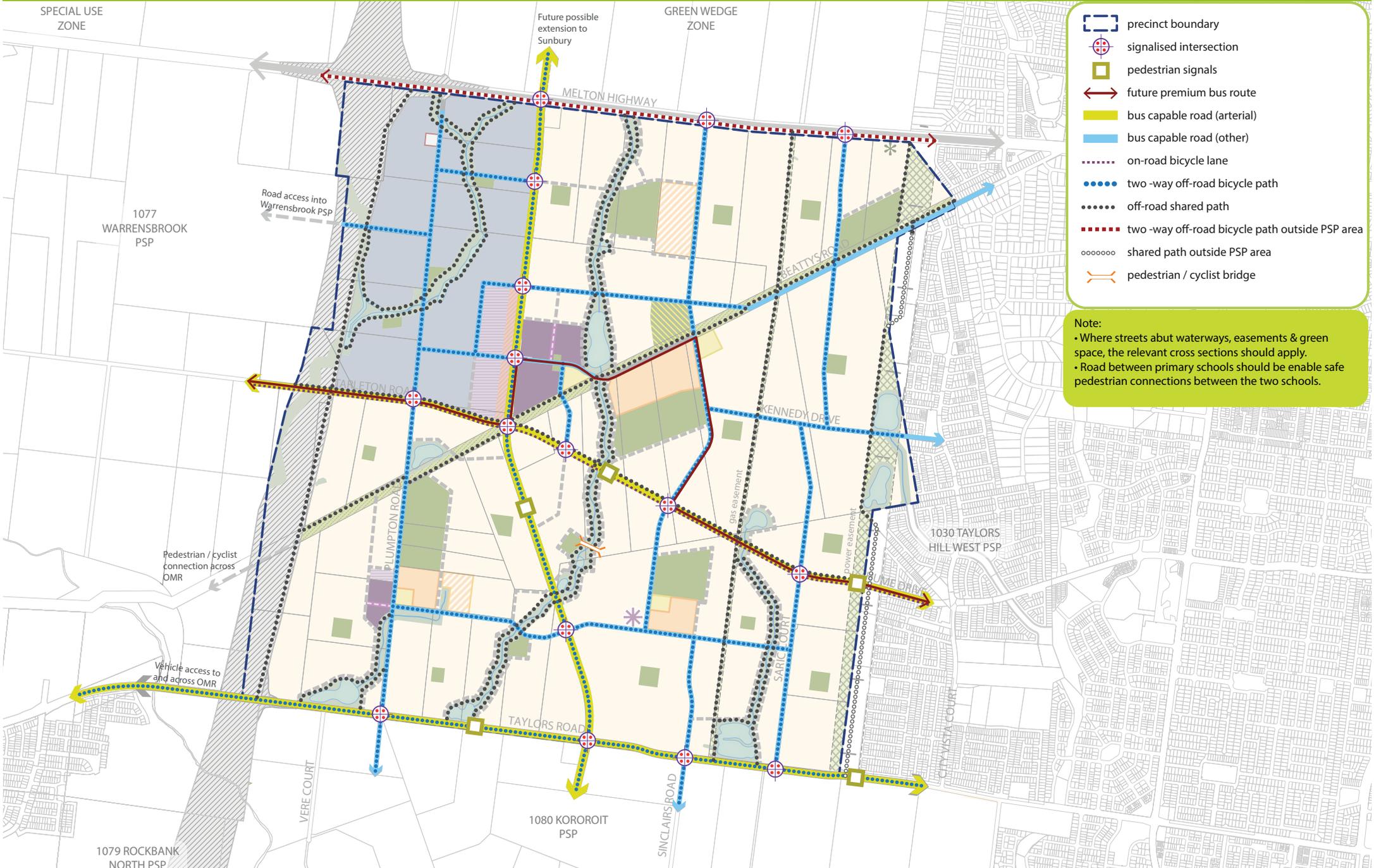
#### 3.5.1 Street network

REQUIREMENTS	
<b>R47</b>	Subdivision layouts must provide: <ul style="list-style-type: none"> <li>• A permeable, direct and safe street network for walking and cycling;</li> <li>• A safe and low speed street network that encourages walking and cycling; and</li> <li>• Convenient access to local points of interest and destinations for effective integration with neighbouring properties.</li> </ul>
<b>R48</b>	Properties abutting the future Hopkins Road must prioritise delivery of the road in the early stages of development, to the satisfaction of the responsible authority.
<b>R49</b>	Properties abutting Melton Highway must deliver interim intersection works to Melton Highway as per Plan 9 in the early stages of development; and a continuous bicycle path within the road reserve; both to the satisfaction of the responsible authority.
<b>R50</b>	Configuration of vehicle access to lots from a public street must ensure that there is sufficient separation between crossovers to allow for a minimum of one on-street car park for every two residential lots and canopy tree planting in accordance with Appendix D.
<b>R51</b>	Vehicle access to lots fronting arterial roads must be provided from a local internal loop road, rear lane, or service road to the satisfaction of the road authority.
<b>R52</b>	Where a lot is 7.5 metres or less in width, vehicle access must be via rear laneway, unless otherwise approved by the responsible authority.
<b>R53</b>	Development must positively address all waterways through the use of frontage roads or lots with a direct frontage and rear access to the satisfaction of Melbourne Water and the responsible authority.
<b>R54</b>	Roundabouts, where determined to be required at cross road intersections, must be designed to reduce vehicle speeds and ensure safe crossings for pedestrians and cyclists and continuity of shared paths and bicycle paths.
<b>R55</b>	Where a connector street crosses a waterway as illustrated on Plan 8, the developer proponent must construct an appropriate vehicle crossing prior to the issue of statement of compliance for the initial stage of subdivision on the opposite side of the waterway, whether or not that residential subdivision directly abuts the waterway, to the satisfaction of the responsible authority and Melbourne Water.
<b>R56</b>	Road networks and street types must be designed and developed in accordance with the street cross sections in Appendices D-F unless otherwise agreed by the responsible authority.

<b>R57</b>	Alternative street cross sections such as illustrated in Appendix D must be to the satisfaction of the responsible authority and ensure that: <ul style="list-style-type: none"> <li>• Minimum required carriageway dimensions are maintained to ensure safe and efficient operation of emergency vehicles on all streets as well as buses on connector streets;</li> <li>• The performance characteristics of standard street cross sections as they relate to pedestrian and cycling use are maintained; and</li> <li>• Relevant minimum road reserve widths for the type of street are maintained as illustrated in Appendix D – Road Cross Sections.</li> </ul>
<b>R58</b>	Any changes required to the design of roads and intersections in the PSP at the time of development must be accommodated within the land take identified in Plan 4 and Appendix A.
<b>R59</b>	Where a local access street is determined to be required to cross a waterway (Service Open Space (Drainage)), the proponent must construct a local access street culvert to the satisfaction of the responsible authority.
GUIDELINES	
<b>G49</b>	Approximately 30% of streets (including connector streets) within a subdivision should apply an alternative cross section to the standard street cross sections outlined in Appendix D, to the satisfaction of the responsible authority. Examples of potential variations are provided in Appendix D. Other non-standard variations are encouraged regarding, but not limited to: <ul style="list-style-type: none"> <li>• Varied street tree placement</li> <li>• Varied footpath or carriageway placement</li> <li>• Varied carriageway or parking bay pavement material</li> <li>• Introduction of elements to create a boulevard effect</li> <li>• Differing tree outstand treatments.</li> </ul>
<b>G50</b>	Street layouts should provide multiple convenient routes to key destinations such as schools, community facilities, sports reserves, Plumpton Major Town Centre, the Business and Industrial Precinct and the Local Town Centre.
<b>G51</b>	Street block lengths should not exceed 240 metres to ensure a safe, permeable and low speed environment for pedestrians, cyclists and vehicles is achieved.
<b>G52</b>	Culs-de-sac should not detract from convenient pedestrian, cycle and vehicular connections.
<b>G53</b>	The frequency of vehicular crossovers on widened verges (in excess of six metres) should be minimised through the use of a combination of: <ul style="list-style-type: none"> <li>• Rear loaded lots with laneway access</li> <li>• Vehicular access from the side of a lot</li> <li>• Combined or grouped crossovers</li> <li>• Increased lot widths.</li> </ul>

<b>G54</b>	Slip lanes should be avoided in areas of high pedestrian activity and only be provided at any intersection between connector streets and arterial roads where they are necessitated by unusually high traffic volumes, and to the satisfaction of the coordinating road authority.
<b>G55</b>	Alignment of future primary arterial roads may be altered so long as the intended performance and function of the roads are maintained to the satisfaction of the coordinating road authority and in consultation with affected landowners.
<b>G56</b>	Streets should be the primary interface between development and waterways, with open space and lots with a direct frontage allowed only as a minor component of the waterway interface.
<b>G57</b>	Where lots with direct frontage are provided, they should be set back five metres from the waterway corridor (as defined in Appendix E) to provide pedestrian and service vehicle access to those lots, to the satisfaction of Melbourne Water and the responsible authority.
<b>G58</b>	All signalised intersections should be designed in accordance with VicRoads' <i>Growth Area Road Network Planning Guidance &amp; Policy Principles</i> (2015).

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**Legend:**

- precinct boundary
- signalised intersection
- pedestrian signals
- future premium bus route
- bus capable road (arterial)
- bus capable road (other)
- on-road bicycle lane
- two-way off-road bicycle path
- off-road shared path
- two-way off-road bicycle path outside PSP area
- shared path outside PSP area
- pedestrian / cyclist bridge

**Note:**

- Where streets abut waterways, easements & green space, the relevant cross sections should apply.
- Road between primary schools should be enable safe pedestrian connections between the two schools.

### 3.5.2 Public transport

REQUIREMENTS	
<b>R60</b>	The street network must be designed to ensure 95% of all households are located within 400 metres of public transport services, and all households are able to directly and conveniently walk to public transport services.
<b>R61</b>	Subdivision design must enable passive surveillance to the public transport network by designing buildings which front on to streets on the public transport network.
<b>R62</b>	Bus stops must be provided which enable convenient access to Plumpton Major Town Centre, the Business and Industrial Precinct and activity-generating land uses such as schools, community facilities, the Local Town Centre, sports reserves and destinations beyond.
<b>R63</b>	Roads and intersections shown as bus capable on Plan 9 must be constructed to accommodate ultra-low-floor buses to the satisfaction of Public Transport Victoria and the responsible authority.

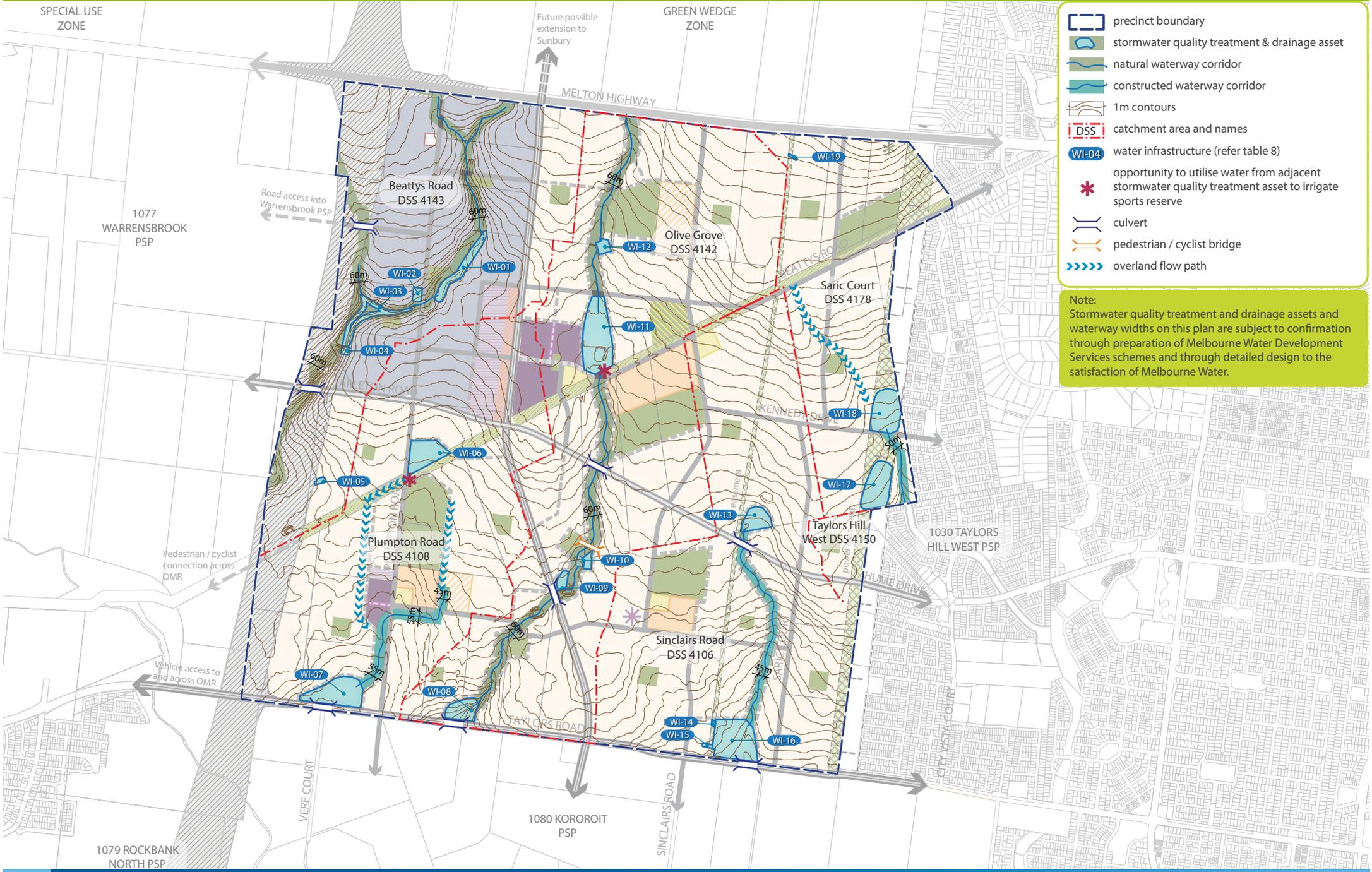
### 3.5.3 Walking and cycling

REQUIREMENTS	
<b>R64</b>	<p>Design of all subdivisions, streets and arterial roads must give priority to the requirements of pedestrians and cyclists by providing:</p> <ul style="list-style-type: none"> <li>• Footpaths of at least 1.5 metres in width on both sides of all streets, roads and bridges unless otherwise specified by the PSP;</li> <li>• Shared paths or bicycle paths of 3.0 metres in width where shown on Plan 9 or as shown on the relevant cross sections illustrated at Appendix D or as specified in another requirement in the PSP;</li> <li>• Safe and convenient crossing points of connector and local streets at all intersections and at key desire lines ;</li> <li>• Pedestrian and cyclist priority crossings on all slip lanes; and</li> <li>• Safe and convenient transition between on- and off-road bicycle networks</li> </ul> <p>All to the satisfaction of the coordinating road authority and the responsible authority.</p>
<b>R65</b>	<p>Pedestrian and cyclist bridges must be provided in accordance with Plan 9 to provide pedestrian and cyclist connectivity throughout the precinct.</p>
<b>R66</b>	<p>Road bridges within the precinct and to areas outside the precinct (such as Hopkins Road Freeway Interchange, and the future Taylors Road crossing of the OMR) must:</p> <ul style="list-style-type: none"> <li>• Include off-road pedestrian and cyclist paths (and/or shared path as relevant); and</li> <li>• Provide sufficient clearance over creeks and waterways to allow for a shared path under the bridge where relevant.</li> </ul>
<b>R67</b>	<p>Shared and pedestrian paths along waterways must be constructed to a standard that satisfies the requirements of Melbourne Water and the responsible authority, and must be:</p> <ul style="list-style-type: none"> <li>• Delivered by development proponents consistent with the network illustrated on Plan 9</li> <li>• Positioned above 1:10 year flood levels with a crossing of the waterway designed above 1:100 year flood level to maintain hydraulic function of the waterway.</li> </ul> <p>Where direct access is provided to the dwelling from the reserve / drainage corridor the path is to be above the 1:100 year flood level.</p>
<b>R68</b>	<p>Safe, accessible and convenient pedestrian and cycle crossing points must be provided at all intersections, key desire lines and locations of high amenity.</p>
<b>R69</b>	<p>Bicycle priority at intersections of minor streets and connector streets with dedicated off-road bicycle paths must be achieved through strong and consistent visual clues and supportive directional and associated road signs.</p>

<b>R70</b>	<p>Alignment of the off-road bicycle path must be designed for cyclists to travel up to 30km/h to the satisfaction of the responsible authority.</p>
<b>R71</b>	<p>Bicycle parking facilities including bicycle hoops and way-finding signage must be provided by development proponents in, and to, key destinations such as Plumpton Major Town Centre, the Business and Industrial precinct, the Local Town Centre, schools, community facilities and across the open space network.</p>
<b>R72</b>	<p>Lighting must be installed along shared, pedestrian and cycle paths linking to key destinations, unless otherwise agreed by the responsible authority.</p>

### 3.5.4 Town centre transport, access and connectivity

REQUIREMENTS	
<b>R73</b>	Heavy vehicle movements (loading and deliveries) must not front the main streets and should be located to the rear and/or side street and screened, or 'sleeved' by more active uses.
<b>R74</b>	Town Centre main streets must be designed for a low speed environment of 30km/h or less such that vehicles and cyclists share the carriageway and pedestrians can safely cross the road.
<b>R75</b>	Increased permeability in the road network within and surrounding the Major Town Centre and Local Town Centre should be delivered via shorter block lengths and the avoidance of culs-de-sac.
<b>R76</b>	Safe and easy access for pedestrian and cycle trips must be provided to town centres through the layout and design of the surrounding street network, including connections to the Beattys Road Reserve linear path and paths along waterways.
GUIDELINES	
<b>G59</b>	Pedestrian priority should be provided across all side roads along main streets and all car park entrances.
<b>G60</b>	Bicycle parking should be provided at entry points to the town centre and designed to include weather protection, passive surveillance and lighting to the satisfaction of the responsible authority.
<b>G61</b>	Car park entrances directly from main streets should be minimised and alternative access should be provided from other streets.
<b>G62</b>	Car parking should be provided efficiently through use of shared, consolidated parking areas.
<b>G63</b>	A safe, clearly identified and continuous path of pedestrian travel should be provided throughout all car parking areas.



- precinct boundary
- stormwater quality treatment & drainage asset
- natural waterway corridor
- constructed waterway corridor
- 1m contours
- catchment area and names
- water infrastructure (refer table 8)
- opportunity to utilise water from adjacent stormwater quality treatment asset to irrigate sports reserve
- culvert
- pedestrian / cyclist bridge
- overland flow path

**Note:**  
Stormwater quality treatment and drainage assets and waterway widths on this plan are subject to confirmation through preparation of Melbourne Water Development Services schemes and through detailed design to the satisfaction of Melbourne Water.

## 4.0 INTEGRATED WATER MANAGEMENT AND UTILITIES

### 4.1 Integrated water management

REQUIREMENTS	
<b>R77</b>	Stormwater runoff from the development must meet or exceed the performance objectives of the CSIRO Best Practice Environmental Management Guidelines for Urban Stormwater prior to discharge to receiving waterways and as outlined on Plan 10, unless otherwise approved by Melbourne Water and the responsible authority.
<b>R78</b>	Final design and boundary of constructed wetlands, retarding basins, stormwater quality treatment infrastructure, and associated paths, boardwalks, bridges, and planting, must be to the satisfaction of both the responsible authority and Melbourne Water.
<b>R79</b>	Development applications must demonstrate how: <ul style="list-style-type: none"> <li>• Waterways and integrated water management design enables land to be used for multiple recreation and environmental purposes;</li> <li>• Overland flow paths and piping within road reserves will be connected and integrated across property/parcel boundaries;</li> <li>• Melbourne Water and the responsible authority freeboard requirements for overland flow paths will be adequately contained within the road reserves;</li> <li>• Relevant integrated water management plan requirements will be achieved to the satisfaction of the water retail authority; and</li> <li>• Melbourne Water drainage assets must be delivered to the satisfaction of Melbourne Water and the responsible authority.</li> </ul>
<b>R80</b>	Development staging must provide for delivery of ultimate waterway and drainage infrastructure including stormwater quality treatment. Where this is not possible, development must demonstrate how any interim solution adequately manages and treats stormwater generated from the development and how this will enable delivery of an ultimate drainage solution, all to the satisfaction of the responsible authority.
<b>R81</b>	Stormwater conveyance and treatment must be designed in accordance with the relevant development services scheme (DSS), Plan 10 and Table 7 to the satisfaction of Melbourne Water and the responsible authority.

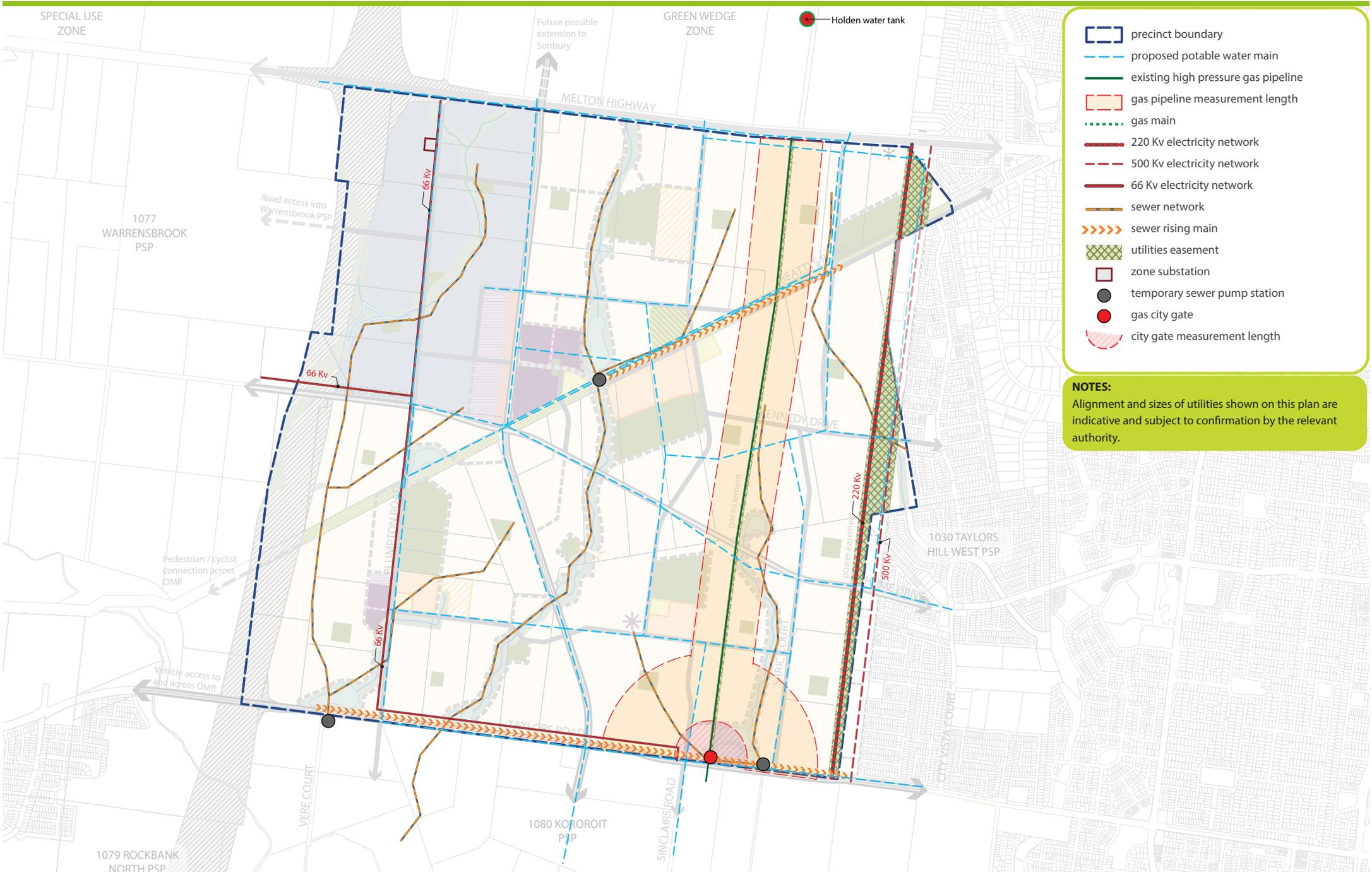
GUIDELINES	
<b>G64</b>	Development should have regard to relevant policies and strategies being implemented by the responsible authority, Melbourne Water, City West Water and Western Water including any approved Integrated Water Management Plan.
<b>G65</b>	Where practical, integrated water management systems should be designed to <ul style="list-style-type: none"> <li>• Maximise habitat values for local flora and fauna species; and</li> <li>• Enable future harvesting and/or treatment and re-use of stormwater, including these options or opportunities outlined on Plan 10.</li> </ul>
<b>G66</b>	The design and layout of roads, road reserves, and public open space should optimise water use efficiency and long-term viability of vegetation and public uses through the use of overland flow paths, Water Sensitive Urban Design initiatives such as rain gardens and/or locally treated storm water for irrigation to contribute to a sustainable and green urban environment.
<b>G67</b>	Development should reduce reliance on potable water by increasing the utilisation of fit-for-purpose alternative water sources such as storm water, rain water and recycled water.

Amended by C203 **Table 7** Stormwater infrastructure

STORMWATER INFRASTRUCTURE ID (REFER TO PLAN 10)	PROPERTY NO.	TYPE	AREA (HA)	KEY ATTRIBUTES	RESPONSIBILITY FOR DELIVERY	DSS
WI-01	12 - E, 13	Wetland	1.70	Associated with geomorphic value waterways with riparian values.	Melbourne Water	Beattys Road
WI-02	5	Sediment Basin and Bio-retention Basin	0.33	-	Melbourne Water	Beattys Road
WI-03	5, 6	Retarding Basin	1.87	-	Melbourne Water	Beattys Road
WI-04	6	Sediment Basin	0.17	-	Melbourne Water	Beattys Road
WI-05	9	Sediment Basin and Bio-retention Basin	0.15	-	Melbourne Water	Beattys Road
WI-06	11 - R	Retarding Basin and Wetland	2.40	Partially located within Beattys Road reservation. Flood overland flows are to be directed from WI-06 and conveyed safely down subdivisional roads to downstream waterways and WI-07.	Melbourne Water	Plumpton Road
WI-07	43	Retarding Basin and Wetland	3.50	-	Melbourne Water	Plumpton Road
WI-08	45, 46	Retarding Basin and Wetland	1.58	-	Melbourne Water	Olive Grove
WI-09	32, 48	Sediment Basin	0.45	-	Melbourne Water	Olive Grove
WI-10	31, 32, 48	Sediment Basin	0.33	-	Melbourne Water	Olive Grove
WI-11	12 - R	Retarding Basin	5.10	Edges to have attractive landscaping and grasses to provide appealing interface with Major Town Centre even when holding minimal water. Embankment of the retarding basin to be clearly separate from road structure in Beattys Road reservation.	Melbourne Water	Olive Grove
WI-12	12-R, 17	Sediment Basin	0.44	-	Melbourne Water	Olive Grove
WI-13	28	Retarding Basin	1.45	Located adjacent to high pressure gas transmission pipeline and gas city gate. Consult with pipeline owner prior to design as per requirements.	Melbourne Water	Sinclair's Road
WI-14	48, 49	Swale	0.06	Located across the high pressure gas transmission pipeline. Consult with pipeline owner prior to design as per requirements.	Melbourne Water	Sinclair's Road
WI-15	48, 49	Swale	0.10	Located across the high pressure gas transmission pipeline. Consult with pipeline owner prior to design as per requirements.	Melbourne Water	Sinclair's Road
WI-16	49, 51	Retarding Basin and Wetland	7.35	Located adjacent to high pressure gas transmission pipeline. Consult with pipeline owner prior to design as per requirements.	Melbourne Water	Sinclair's Road

STORMWATER INFRASTRUCTURE ID (REFER TO PLAN 10)	PROPERTY NO.	TYPE	AREA (HA)	KEY ATTRIBUTES	RESPONSIBILITY FOR DELIVERY	DSS
WI-17	27	Wetland	5.94	Located in powerlines easement. Consult with powerlines easement owner prior to design as per requirements.	Melbourne Water	Saric Court
WI-18	27	Retarding Basin and Wetland	6.20	Located in powerlines easement. Consult with powerlines easement owner prior to design as per requirements. Flood overland flows are to be split and conveyed safely down to WI-18 from the upper catchment north of Beattys Road.	Melbourne Water	Saric Court
WI-19	21, 23	Swale	0.06	Located across the high pressure gas transmission pipeline. Consult with pipeline owner prior to design as per requirements.	Melbourne Water	Saric Court

Note: The areas and corridor widths identified in this table are subject to refinement during detailed design to the satisfaction of Melbourne Water and the responsible authority



- precinct boundary
- proposed potable water main
- existing high pressure gas pipeline
- gas pipeline measurement length
- gas main
- 220 Kv electricity network
- 500 Kv electricity network
- 66 Kv electricity network
- sewer network
- sewer rising main
- utilities easement
- zone substation
- temporary sewer pump station
- gas city gate
- city gate measurement length

**NOTES:**  
 Alignment and sizes of utilities shown on this plan are indicative and subject to confirmation by the relevant authority.

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## 4.2 Utilities

REQUIREMENTS	
<b>R82</b>	Trunk services are to be placed along the general alignments shown on Plan 11 subject to any refinements as advised by the relevant servicing authorities.
<b>R83</b>	<p>Before development commences on a property, functional layout plans of the road network must be submitted that illustrate the location of all:</p> <ul style="list-style-type: none"> <li>• Underground services;</li> <li>• Driveways and crossovers;</li> <li>• Intersection devices;</li> <li>• Shared, pedestrian and bicycle paths;</li> <li>• Street lights; and</li> <li>• Street trees.</li> </ul> <p>A typical cross section of each street must also be submitted showing above- and below-ground placement of services, street lights and trees.</p> <p>The plans and cross sections must demonstrate how services, driveways and street lights will be placed to achieve the required road reserve width (consistent with the road cross sections outlined in Appendix D) and accommodate at least the minimum street tree planting requirements.</p> <p>The plans and cross sections are to be approved by the responsible authority and all relevant service authorities before development commences and may be approved in stages to the satisfaction of the responsible authority.</p>
<b>R84</b>	Delivery of underground services must be coordinated, located and bundled (utilising common trenching) to facilitate tree and other planting within road verges.
<b>R85</b>	All existing above ground electricity cables (excluding substations and cables with voltage 66kv or greater) must be placed underground as part of the upgrade or subdivision of existing roads.
<b>R86</b>	All new electricity supply infrastructure (excluding substations and cables with voltage 66kv or greater) must be provided underground.
<b>R87</b>	Above ground utilities must be identified at the subdivision design stage to ensure effective integration with the surrounding neighbourhood and to minimise amenity impacts, and be designed to the satisfaction of the responsible authority.
<b>R88</b>	Where the infrastructure is intended to be located in public open space, the land required to accommodate that infrastructure will not be counted as contribution to public open space requirements classified under the Plumpton and Kororoit Infrastructure Contributions Plan.

<b>R89</b>	Utilities must be placed outside of conservation areas, natural waterway corridors or on the outer edges of these corridors in the first instance. Where services cannot avoid crossing or being located within a conservation area or natural waterway corridor they must be located to avoid disturbance to existing waterway values, native vegetation, areas of strategic importance to Growling Grass Frog, significant landform features and heritage sites, to the satisfaction of the Department of Environment, Land, Water and Planning, Melbourne Water and the responsible authority.
<b>R90</b>	All temporary infrastructure must be removed once permanent infrastructure is connected and operating.
GUIDELINES	
<b>G68</b>	Above-ground utilities, including temporary utilities, should be located outside of key view lines and screened with vegetation, as appropriate.

## 5.0 INFRASTRUCTURE DELIVERY & STAGING

### 5.1 Subdivision works by developers

REQUIREMENTS	
<b>R91</b>	<p>Subdivision of land within the precinct must provide and meet the total cost of delivering the following infrastructure (other than where indicated otherwise in Plan 12 and Plan 13):</p> <ul style="list-style-type: none"> <li>• Connector streets and local streets;</li> <li>• Local bus stop infrastructure (where locations have been agreed in writing by Public Transport Victoria);</li> <li>• Landscaping of all existing and future roads and local streets, including canopy tree planting;</li> <li>• Intersection works and traffic management measures along arterial roads, connector streets, and local streets;</li> <li>• Local shared, pedestrian and bicycle paths along local roads, connector streets, utilities easements, local streets, waterways and within local parks including bridges, intersections, and barrier crossing points;</li> <li>• Council-approved fencing and landscaping along arterial roads, where required;</li> <li>• Bicycle parking;</li> <li>• Appropriately scaled lighting along all roads and major shared and pedestrian paths across the open space network;</li> <li>• Basic improvements to local parks and open space as outlined in this PSP;</li> <li>• Local drainage system;</li> <li>• Connector and local street or pedestrian/cycle path crossings of waterways;</li> <li>• Infrastructure as required by utility services providers, including water, sewerage, drainage (except where the item is funded through a DSS), electricity, gas and telecommunications; and</li> <li>• Remediation and/or reconstruction of dry stone walls, where required.</li> </ul>

<b>R92</b>	<p>All public open space (other than where improvements are included in Table 8) must be finished to a standard that satisfies the requirements of the responsible authority prior to the transfer of the public open space, including but not limited to:</p> <ul style="list-style-type: none"> <li>• Removal of all existing disused structures, foundations, pipelines and stockpiles;</li> <li>• Clearing of rubbish and environmental weeds and rocks, levelled, topsoiled and grassed with warm climate grass;</li> <li>• Provision of water tapping, potable and recycled water connection points (where a recycled water supply is required by the relevant water authority);</li> <li>• Sewer, gas and electricity connection points to land identified as sports reserves and community facilities;</li> <li>• Trees and other plantings;</li> <li>• Vehicle exclusion devices (fence, bollards or other suitable methods) and maintenance access points;</li> <li>• Construction of pedestrian paths to a minimum 1.5 metres in width around the perimeter of the reserve and connecting to the surrounding path network (and/or a 3.0m wide shared path where required by Plan 10 and connecting to the surrounding path network);</li> <li>• Installation of park furniture, including barbecues, shelters, furniture, rubbish bins, local-scale play areas, and appropriate paving to support these facilities, consistent with the type of open space listed in Table 8 and Appendix I, and in accordance with any relevant adopted Council open space/ landscape document; and</li> <li>• Removal of any soil contamination.</li> </ul>
<b>R93</b>	<p>Local sports reserves identified in Table 6 must be vested in the relevant authority in the following condition:</p> <ul style="list-style-type: none"> <li>• Free from surface and/or protruding rocks and structures;</li> <li>• Reasonably graded and/or topsoiled to create a safe and regular surface with a maximum 1:6 gradient;</li> <li>• Seeded and top-dressed with drought-resistant grass in bare, patchy and newly-graded areas; and</li> <li>• Removal of any soil contamination.</li> </ul>
<b>R94</b>	<p>Convenient and direct access to the connector road network must be provided through neighbouring properties where a property does not otherwise have access to the connector network or signalled access to the arterial road network, as appropriate.</p>
<b>R95</b>	<p>Where a street has already been constructed or approved for construction to a property boundary, subsequent development must connect with that street to adopt a consistent cross-section until a suitable transition can be made.</p>

## 5.2 Development staging

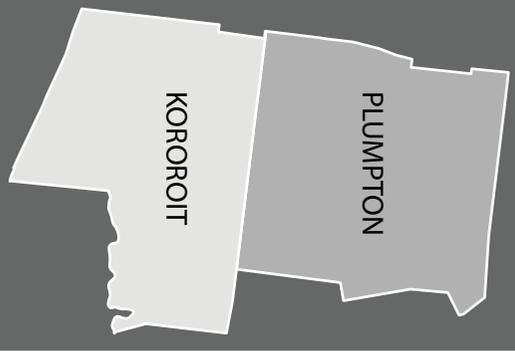
REQUIREMENTS	
<b>R96</b>	<p>Development staging must provide for the timely provision and delivery of:</p> <ul style="list-style-type: none"> <li>• Arterial road reservations;</li> <li>• Connector streets;</li> <li>• Street links between properties, constructed to the property boundary; and</li> <li>• Connection of the on- and off-road pedestrian and bicycle network.</li> </ul>
<b>R97</b>	<p>Streets must be constructed to property boundaries where an inter-parcel connection is intended or indicated in this precinct structure plan, by any date or stage of development required or approved by the responsible authority.</p>
<b>R98</b>	<p>Staging will be determined largely by the development proposals on land within the precinct and the availability of infrastructure services. Within this context, development applications must demonstrate how the development will:</p> <ul style="list-style-type: none"> <li>• Integrate with adjoining developments, including the timely provision of road and path connections, to the extent practical;</li> <li>• Integrate with other developments, including the timely provision of road and path connections to the extent practical, where the proposed development does not adjoin an existing development front;</li> <li>• Provide sealed road access to each new allotment;</li> <li>• Provide open space and amenity to new residents in the early stages of the development, where relevant; and</li> <li>• Deliver any necessary trunk service extensions, including confirmation of agreed approach and timing by the relevant authority.</li> </ul>
GUIDELINES	
<b>G69</b>	<p>Staging of transport infrastructure should prioritise early delivery of a connected arterial road network to:</p> <ul style="list-style-type: none"> <li>• Ensure that subdivisions are designed to access the future arterial network rather than the existing road network</li> <li>• Reduce pressure on existing roads which were built to cater for rural, not urban use</li> <li>• Reduce pressure on the existing low standard crossing of Kororoit Creek at Sinclairs Road in the Kororoit PSP area to the south.</li> </ul>
<b>G70</b>	<p>The early delivery of community facilities, local parks and playgrounds is encouraged within each neighbourhood and may be delivered in stages, to the satisfaction of the responsible authority.</p>

## 6.0 PRECINCT INFRASTRUCTURE

The Precinct Infrastructure Table at Table 8 sets out the infrastructure and services required to meet the needs of proposed development within the precinct, as illustrated on Plans 12 and 13. Indicative timing is designated as 'S' (short term); 'M' (medium term); and 'L' (long term). The infrastructure items and services are to be provided through a number of mechanisms which may include:

- Subdivision construction works by developers;
- Agreement under Section 173 of the Act;
- Utility service provider requirements;
- The Plumpton and Kororoit ICP;
- Relevant development contributions from adjoining areas;
- Capital works projects by Council, State government agencies and non-government organisations; and
- Works In Kind (WIK) projects undertaken by developers on behalf of Council or State government agencies.

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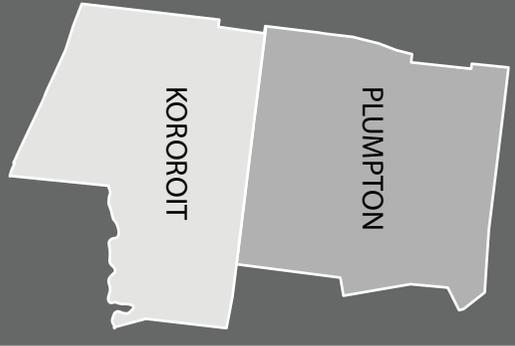
	ICP boundary
	road project
	intersection project
	bridge project
	culvert project
	pedestrian bridge
	pedestrian signal
	construction of road delivered by Taylors Hill West DCP and habitat compensation obligations included in the ICP

	ICP project identification number
	BR bridge
	PBR pedestrian bridge
	CU culvert
	IN intersection
	RD road
	PS pedestrian signal



INSET MAP



	ICP boundary
	community facilities project
	indoor recreation
	local sports reserve
	local park
	conservation
	conservation easement
	future government school
	potential non-government school
	ICP project identification number
	GS future government school
	NGS potential non-government school
	LP local park
	SR sports reserve
	CI community infrastructure
	IR indoor recreation
	LOS local open space
	CA conservation

Amended by C203 **Table 8** Precinct infrastructure

As there will be a single Plumpton and Kororoit ICP, ICP projects in both PSPs are listed in Table 8. Items located partly or wholly on properties in Plumpton PSP are highlighted in blue shaded rows in Table 8.

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
<b>TRANSPORT PROJECTS</b>						
<b>ROAD PROJECTS</b>						
Road	RD-01	Hopkins Road: Melton Highway (IN-01) to East-West Road (IN-04)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard). Construction of a 6-lane arterial road (ultimate standard).	Yes No	Melton City Council VicRoads	L L
Road	RD-02	Hopkins Road: East-West Road 1 (IN-04) to East-West 2 Road (IN-05)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard). Construction of a 6-lane arterial road (ultimate standard).	Yes No	Melton City Council VicRoads	L L
Road	RD-03	Hopkins Road: East-West Road 2 (IN-05) to East-West Road 3 (IN-06)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard). Construction of a 6-lane arterial road (ultimate standard).	Yes No	Melton City Council VicRoads	L L
Road	RD-04	Hopkins Road: Tarleton Road (IN-08) to East-West Road 4 (IN-12)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard). Construction of a 6-lane arterial road (ultimate standard).	Yes No	Melton City Council VicRoads	L L
Road	RD-05	Hopkins Road: East-West Road 4 (IN-12) to Taylors Road (IN-14)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard). Construction of a 6-lane arterial road (ultimate standard)	Yes No	Melton City Council VicRoads	L L
Road	RD-06	Tarleton Road: Western PSP Boundary to Plumpton Road (IN-07)	Provision of land to construct new road reserve 34m wide (ultimate standard), construction of a 2-lane arterial road (interim standard) and habitat compensation obligations. Construction of a 4-lane arterial road (ultimate standard).	Yes No	Melton City Council Melton City Council	L L
Road	RD-07	Tarleton Road: Plumpton Road (IN-07) to Hopkins Road (IN-08)	Provision of land to construct new road reserve 34m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard). Construction of a 4-lane arterial road (ultimate standard).	Yes No	Melton City Council Melton City Council	L L
Road	RD-08	Tarleton Road: North-South Road 3 (IN-09) to North-South Road 4 (IN-10)	Provision of land to construct new road reserve 34m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard). Construction of a 4-lane arterial road (ultimate standard).	Yes No	Melton City Council Melton City Council	L L

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Road	RD-09	Tarleton Road: North-South Road 4 (IN-10) to North-South Road 5 (IN-11)	Provision of land to construct new road reserve 34m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard).	Yes	Melton City Council	L
			Construction of a 4-lane arterial road (ultimate standard).	No	Melton City Council	L
Road	RD-10	Tarleton Road: North-South Road 5 (IN-11) to Hume Drive/eastern PSP boundary	Provision of land to construct new road reserve 34m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard).	Yes	Melton City Council	S
			Construction of a 4-lane arterial road (ultimate standard).	No	Melton City Council	L
Road	RD-11	Taylors Road: Western PSP Boundary to Plumpton Road (IN-13)	Provision of land to construct new road reserve 41m wide (ultimate standard), construction of a 2-lane arterial road (interim standard) and habitat compensation obligations.	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-12	Taylors Road: Plumpton Road (IN-13) to Hopkins Road (IN-14)	Provision of land to construct new road reserve 44m wide (ultimate standard), construction of a 2-lane arterial road (interim standard) and habitat compensation obligations.	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-13	Taylors Road: Hopkins Road (IN-14) to Sinclairs Road (IN-15)	Provision of land to construct new road reserve 44m wide (ultimate standard), construction of a 2-lane arterial road (interim standard) and habitat compensation obligations.	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-14	Taylors Road: Sinclairs Road (IN-15) to Saric Court (IN-16)	Provision of land to construct new road reserve 44m wide (ultimate standard), construction of a 2-lane arterial road (interim standard) and habitat compensation obligations.	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-15	Taylors Road: Saric Court (IN-16)	Provision of land to construct new road reserve 44m wide (ultimate standard).	Yes	Melton City Council	M
			Construction of a 2-lane arterial road (interim standard) from IN-16 to the eastern boundary of the Plumpton PSP.	Yes	Melton City Council	M
			Payment of habitat compensation obligations.	Yes	Melton City Council	M
			Construction of a 2-lane arterial road (interim standard) from the eastern boundary of the Plumpton PSP to IN-17 is 100% funded by Taylors Hill West DCP Project DI-RO-04a.	No	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Road	RD-16	Taylors Road: City Vista Court (IN-17) to Eastern PSP Boundary	Provision of land to construct new road reserve 44m wide (ultimate standard).	Yes	Melton City Council	M
			Payment of habitat compensation obligations.	Yes	Melton City Council	M
			Construction of a 2-lane arterial road (interim standard) is 100% funded by Taylors Hill West DCP Project ID DI-RO-04a.	No	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-17	Hopkins Road: Taylors Road (IN-14) to East-West Road 5 (IN-18)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard).	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-18	Hopkins Road: East-West Road 5 (IN-18) to Reed Court (IN-19)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard).	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-19	Hopkins Road: Reed Court (IN-19) to Hopkins Road Bridge (BR-02)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard).	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-20	Hopkins Road: Hopkins Road Bridge (BR-02) to East-West Road 6 (IN-20)	Provision of land to construct new road reserve 41m wide (ultimate standard) and construction of a 2-lane arterial road (interim standard).	Yes	Melton City Council	M
			Construction of a 6-lane arterial road (ultimate standard).	No	VicRoads	L
Road	RD-21	Neale Road: Sinclairs Road (IN-24) to Clarke Road/Neale Road roundabout	Construction of a 2-lane arterial road (interim standard). Note construction cost 75% externally apportioned to Kororoit Part 2 ICP.	Yes	Melton City Council	L
			Construction of a 2-lane arterial road (ultimate standard).	No	Melton City Council	L
<b>INTERSECTION PROJECTS</b>						
Intersection	IN-01	Intersection: Hopkins Road and Melton Highway	Provision of land (ultimate standard - Hopkins Road only), construction of primary arterial to primary arterial signalised T intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-02	Intersection: North-South Road 2 and Melton Highway	Provision of land (ultimate standard - Hopkins Road only), construction of primary arterial to primary arterial signalised T intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	S
			Construction of signalised T intersection (ultimate standard).	No	VicRoads	L

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PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Intersection	IN-03	Intersection: North-South Road 1 and Melton Highway	Provision of land (ultimate standard - Hopkins Road only), construction of primary arterial to primary arterial signalised T intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	L
			Construction of signalised T intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-04	Intersection: Hopkins Road and East-West Road 1	Provision of land (ultimate standard) and construction of primary arterial to connector road/local road signalised 4-way intersection (interim standard).	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-05	Intersection: Hopkins Road and East-West Road 2	Provision of land (ultimate standard) and construction of primary arterial to connector road signalised 4-way intersection (interim standard).	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-06	Intersection: Hopkins Road and East-West Road 3	Provision of land (ultimate standard) and construction of primary arterial to connector road signalised 4-way intersection (interim standard).	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-07	Intersection: Plumpton Road and Tarleton Road	Provision of land (ultimate standard), construction of connector road to secondary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	Melton City Council	L
Intersection	IN-08	Intersection: Hopkins Road and Tarleton Road	Provision of land (ultimate standard), construction of primary arterial to secondary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-09	Intersection: North-South Road 3 and Tarleton Road	Provision of land (ultimate standard) and construction of connector road/local road to secondary arterial signalised 4-way intersection (interim standard).	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	Melton City Council	L
Intersection	IN-10	Intersection: North-South Road 4 and Tarleton Road	Provision of land (ultimate standard) and construction of connector road to secondary arterial signalised 4-way intersection (interim standard).	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	Melton City Council	L

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Intersection	IN-11	Intersection: North-South Road 5 and Tarleton Road	Provision of land (ultimate standard) and construction of connector road to secondary arterial signalised 4-way intersection (interim standard).	Yes	Melton City Council	S
			Construction of signalised 4-way intersection (ultimate standard).	No	Melton City Council	L
Intersection	IN-12	Intersection: Hopkins Road and East West Road 4	Provision of land (ultimate standard) and construction of primary arterial to connector road signalised 4-way intersection (interim standard).	Yes	Melton City Council	L
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-13	Intersection: Plumpton Road and Taylors Road	Provision of land (ultimate standard), construction of connector road to primary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	S
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-14	Intersection: Hopkins Road and Taylors Road	Provision of land (ultimate standard), construction of primary arterial to primary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	M
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-15	Intersection: Sinclairs Road and Taylors Road	Provision of land (ultimate standard), construction of connector road to primary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	S
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-16	Intersection: Saric Court and Taylors Road	Provision of land (ultimate standard), construction of connector road to primary arterial signalised T intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	M
			Construction of signalised T intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-17	Intersection: City Vista Court and Taylors Road	Provision of land (ultimate standard), construction of connector road to primary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations. Note, 16% of the project construction cost is funded via the Taylors Hill West DCP.	Yes	Melton City Council	M
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-18	Intersection: Hopkins Road and East-West Road 5	Provision of land (ultimate standard), construction of connector road to primary arterial signalised 4-way intersection (interim standard).	Yes	Melton City Council	M
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Intersection	IN-19	Intersection: Hopkins Road and Reed Court	Provision of land (ultimate standard), construction of connector road to primary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	M
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-20	Intersection: Hopkins Road and East-West Road 6	Provision of land (ultimate standard) and construction of connector road to primary arterial signalised 4-way intersection (interim standard).	Yes	Melton City Council	M
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-21	Intersection: Hopkins Road and East-West Local Access Street	Provision of land (ultimate standard) and construction of primary arterial to town centre main street signalised T intersection (interim standard).	Yes	Melton City Council	M
			Construction of signalised T intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-22	Intersection: Hopkins Road and Neale Road	Provision of land (ultimate standard), construction of secondary arterial/connector road to primary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	S
			Construction of signalised 4-way intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-23	Intersection: North-South Local Access Street and Neale Road	Provision of land (ultimate standard), construction of local access street to secondary arterial signalised T intersection (interim standard) and habitat compensation obligations.	Yes	Melton City Council	L
			Construction of a signalised T intersection (ultimate standard).	No	VicRoads	L
Intersection	IN-24	Intersection: Sinclairs Road and Neale Road	Provision of land (ultimate standard) within the Plumpton and Kororoit ICP area, construction of connector road (north leg) / local access street (south leg) to secondary arterial signalised 4-way intersection (interim standard) and habitat compensation obligations. Note construction cost 50% externally apportioned to Kororoit Part 2 ICP.	Yes	Melton City Council	S
			Construction of signalised 4-way intersection (ultimate standard).	No	Melton City Council	L
<b>PEDESTRIAN SIGNALS PROJECTS</b>						
Pedestrian Signals	PS-01	Pedestrian Signals	Construction of pedestrian signals on Hopkins Road single carriageway (interim standard) as part of RD-04.	Yes	Melton City Council	L
			Upgrade of pedestrian signals on Hopkins Road (ultimate standard) as part of RD-04.	No	VicRoads	L
Pedestrian Signals	PS-02	Pedestrian Signals	Construction of pedestrian signals on Tarleton Road single carriageway (interim standard) as part of RD-08.	Yes	Melton City Council	L
			Upgrade of pedestrian signals on Tarleton Road (ultimate standard) as part of RD-08.	No	Melton City Council	L

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PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Pedestrian Signals	PS-03	Pedestrian Signals	Construction of pedestrian signals on Tarleton Road single carriageway (interim standard) as part of RD-10.	Yes	Melton City Council	S
			Upgrade of pedestrian signals on Tarleton Road (ultimate standard) as part of RD-10.	No	Melton City Council	L
Pedestrian Signals	PS-04	Pedestrian Signals	Construction of pedestrian signals on Taylors Road single carriageway (interim standard) as part of RD-12.	Yes	Melton City Council	M
			Upgrade of pedestrian signals on Taylors Road (ultimate standard) as part of RD-12.	No	VicRoads	L
Pedestrian Signals	PS-05	Pedestrian Signals	Construction of pedestrian signals on Taylors Road single carriageway (interim standard) as part of RD-15.	Yes	Melton City Council	M
			Upgrade of pedestrian signals on Taylors Road (ultimate standard) as part of RD-15.	No	VicRoads	L
Pedestrian Signals	PS-06	Pedestrian Signals	Construction of pedestrian signals on Hopkins Road single carriageway (interim standard) as part of RD-20.	Yes	Melton City Council	M
			Upgrade of pedestrian signals on Hopkins Road (ultimate standard) as part of RD-20.	No	VicRoads	L
<b>BRIDGE PROJECTS</b>						
Bridge	BR-01	Vere Court Bridge	Provision of land and construction of a connector road bridge over the Kororoit Creek and in accordance with the Growling Grass Frog Conservation Area requirements.	Yes	Melton City Council	L
Bridge	BR-02	Hopkins Road Bridge	Provision of land (ultimate standard) and construction of a primary arterial road bridge (interim standard) over the Kororoit Creek and in accordance with the Growling Grass Frog Conservation Area requirements.	Yes	Melton City Council	M
			Upgrade of a primary arterial road bridge (ultimate standard).	No	VicRoads	L
Bridge	BR-03	Sinclairs Road Bridge	Construction of a connector road bridge over the Kororoit Creek and habitat compensation obligations.	Yes	Melton City Council	S
Pedestrian/ Cyclist Bridge	PBR-01	Culvert - Pedestrian/cyclist waterway crossing	Construction of pedestrian/cyclist bridge across a natural waterway associated with the Olive Grove DSS.	Yes	Melton City Council	L
Pedestrian/ Cyclist Bridge	PBR-02	Pedestrian/cyclist bridge over Kororoit Creek	Construction of a pedestrian/cyclist bridge across the Kororoit Creek in accordance with Growling Grass Frog Conservation Area requirements.	Yes	Melton City Council	L

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PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Pedestrian/ Cyclist Bridge	PBR-03	Pedestrian/cyclist bridge over Kororoit Creek	Construction of a pedestrian/cyclist bridge across the Kororoit Creek in accordance with Growling Grass Frog Conservation Area requirements. Note construction cost 10% externally apportioned to Kororoit Part 2 ICP.	Yes	Melton City Council	L
Pedestrian/ Cyclist Bridge	PBR-04	Pedestrian/cyclist bridge over Western Freeway	Construction of a pedestrian/cyclist bridge across the Western Freeway to the future proposed Mt Atkinson station. Note construction cost 50% externally apportioned to Mt Atkinson & Tarneit Plains ICP.	Yes	Melton City Council in consultation with VicRoads	L
<b>CULVERT PROJECTS</b>						
Culvert	CU-01	East-West Road 5 Drainage Culvert	Construction of a culvert at waterway associated with the Beattys Road DSS (ultimate standard).	Yes	Melton City Council	L
Culvert	CU-02	Tarleton Road Drainage Culvert	Upgrade of existing culvert at waterway associated with the Beattys Road DSS.	No	Melbourne Water	L
			Construction of a culvert to the interim road width as part of the construction of RD-06.	Yes	Melton City Council	L
Culvert	CU-03	Tarleton Road Drainage Culvert	Construction of a culvert to the ultimate road width as part of the upgrade of RD-06.	No	Melton City Council	L
			Construction of existing culvert at waterway associated with the Olive Grove DSS. To be constructed as part of RD-08 (interim standard).	Yes	Melton City Council	L
Culvert	CU-04	Tarleton Road Drainage Culvert	Construction of a culvert as part of the construction of RD-08 (ultimate standard).	No	Melton City Council	L
			Construction of a culvert at waterway associated with the Sinclairs Road DSS. To be constructed as part of RD-09 (interim standard).	Yes	Melton City Council	L
Culvert	CU-05	Hopkins Road Drainage Culvert	Construction of a culvert as part of RD-09 (ultimate standard).	No	Melton City Council	L
			Construction of a culvert at waterway associated with the Olive Grove DSS. To be constructed as part of RD-04 (interim standard).	Yes	Melton City Council	L
Culvert	CU-06	Taylors Road Drainage Culvert	Construction of culvert as part of the construction of RD-04 (ultimate standard).	No	VicRoads	L
			Upgrade of existing culvert at waterway associated with the Plumpton Road DSS.	No	Melbourne Water	M
			Construction of culvert to the interim road width as part of the construction of RD-11 (interim standard).	Yes	Melton City Council	M
			Construction of culvert to the ultimate road width as part of the construction of RD-11.	No	VicRoads	L

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PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Culvert	CU-07	Taylors Road Drainage Culvert	Upgrade of existing culvert at waterway associated with the Olive Grove DSS.	No	Melbourne Water	M
			Construction of culvert to the interim road width as part of the construction of RD-12.	Yes	Melton City Council	M
			Construction of culvert to the ultimate road width as part of the construction of RD-12.	No	VicRoads	L
Culvert	CU-08	Taylors Road Drainage Culvert	Upgrade of existing culvert at waterway associated with the Sinclairs Road DSS.	No	Melbourne Water	M
			Construction of culvert to the interim road width as part of the construction of RD-14.	Yes	Melton City Council	M
			Construction of culvert to the ultimate road width as part of the upgrade of RD-14.	No	VicRoads	L
<b>PUBLIC TRANSPORT PROJECTS</b>						
Public Transport	-	Bus services	Delivery of bus services	No	Public Transport Victoria	S (part delivery)
<b>COMMUNITY FACILITIES AND OPEN SPACE</b>						
<b>COMMUNITY FACILITY PROJECTS</b>						
Community	CI-01	Plumpton Community Centre & Neighbourhood House	Provision of land and construction of a multi-purpose community centre (Level 2) and neighbourhood house facilities. This will include community rooms, kindergarten and maternal health, youth space, additional classroom space and specialist facilities.	Yes	Melton City Council	S
Community	CI-02	Multi Purpose Community Centre (with Library)	Provision of land and construction of a multi-purpose community centre (Level 3), library, and neighbourhood house facilities.	Yes	Melton City Council	L
Community	CI-03	Plumpton West Community Centre	Provision of land and construction of a multi-purpose community centre (Level 1) including community rooms and additional facilities to cater for kindergarten and maternal health.	Yes	Melton City Council	L
Community	CI-04	Plumpton East Community Centre	Provision of land and construction of a multi-purpose community centre (Level 1) including community rooms and additional facilities to cater for kindergarten and maternal health.	Yes	Melton City Council	L
Community	CI-05	Deanside Community Centre & Neighbourhood House	Provision of land and construction of a multi-purpose community centre (Level 2) and neighbourhood house facilities. This will include community rooms, kindergarten and maternal health, youth space, additional classroom space and specialist facilities.	Yes	Melton City Council	L

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PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Community	CI-06	Kororoit Community Centre	Provision of land and construction of a multi-purpose community centre (Level 1) including community rooms and additional facilities to cater for kindergarten and maternal health.	Yes	Melton City Council	L
Community	CI-07	Kororoit East Community Centre	Provision of land and construction of a multi-purpose community centre (Level 1) including community rooms and additional facilities to cater for kindergarten and maternal health.	Yes	Melton City Council	S
<b>INDOOR RECREATION PROJECTS</b>						
Community	IR-01	Plumpton Aquatics Centre	Provision of land for an aquatics centre. Note: provision of land is 50% externally apportioned to Melton City Council.	Yes	Melton City Council	L
			Construction of an aquatics centre.	No	Melton City Council	L
Community	IR-02	Deanside Indoor Recreation Facility	Provision of land for an indoor recreation facility.	Yes	Melton City Council	L
			Construction of an indoor recreation facility.	No	Melton City Council	L
<b>SPORTS RESERVE PROJECTS</b>						
Sports Reserve	SR-01	Plumpton North Sports Reserve	Provision of land construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A pavilion to serve the Plumpton North sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• A tennis/multipurpose hard courts facility incorporating 6 courts with lighting, parking, including all construction works, landscaping and related infrastructure.</li> <li>• Playground including playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	L
Sports reserve	SR-02	Plumpton East Sports Reserve	Povision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A Pavilion to serve the Plumpton East sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• Playground including playspace, youth space, picnic and BBQ.</li> <li>• Habitat compensation obligations.</li> </ul>	Yes	Melton City Council	L

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PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Sports reserve	SR-03	Plumpton Sports Reserve	Provision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A pavilion to serve the Plumpton sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• Playground including large playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	M
Sports reserve	SR-04	Plumpton West Sports Reserve	Provision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A pavilion to serve the Plumpton West sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• Playground including playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	L
Sports reserve	SR-05	Plumpton South Sports Reserve	Provision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A pavilion to serve the Plumpton South sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• A tennis/multipurpose hard courts facility incorporating 8 courts with lighting, parking, including all construction works, landscaping and related infrastructure.</li> <li>• Playground including playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	L
Sports reserve	SR-06	Deanside Sports Reserve	Provision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A pavilion to serve the Deanside sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• Playground including large playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	L
Sports reserve	SR-07	Kororoit West Sports Reserve	Provision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A pavilion to serve the Kororoit West sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• Playground including playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	L

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Sports reserve	SR-08	Kororoit Sports Reserve	Provision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping, large playground and related infrastructure.</li> <li>• A pavilion to serve the Kororoit sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• A tennis/multipurpose hard courts facility incorporating 6 courts with lighting, parking, including all construction works, landscaping and related infrastructure.</li> <li>• Playground including playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	L
Sports reserve	SR-09	Kororoit East Sports Reserve	Provision of land and construction of a sports reserve incorporating: <ul style="list-style-type: none"> <li>• Playing surfaces and car parks, including all construction works, landscaping and related infrastructure.</li> <li>• A pavilion to serve the Kororoit sports reserve, including all building works, landscaping and related infrastructure.</li> <li>• Playground including playspace, youth space, picnic and BBQ.</li> </ul>	Yes	Melton City Council	M

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**OPEN SPACE AND LOCAL PARK PROJECTS**

Open Space	LP-01	Local Park	Provision of land for a local park.	Yes	Melton City Council	L
			Construction and embellishment of local park.	No	Developer works	L
Open Space	LP-02	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-03	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-04	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-05	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-06	Local Park	Provision of land for a local park.	Yes	Melton City Council	L
			Construction and embellishment of local park.	No	Developer works	L

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Open Space	LP-07	Local Park	Provision of land for a local park	Yes	Melton City Council	S
			Construction and embellishment of local park	No	Developer works	S
Open Space	LP-08	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-09	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-10	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-11	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-12	Local Park	Provision of land for a local park	Yes	Melton City Council	S
			Construction and embellishment of local park	No	Developer works	S
Open Space	LP-13	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-14	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-15	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-16	Local Park	Provision of land for a local park	Yes	Melton City Council	M
			Construction and embellishment of local park	No	Developer works	M

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Open Space	LP-17	Local Park	Provision of land for a local park	Yes	Melton City Council	M
			Construction and embellishment of local park	No	Developer works	M
Open Space	LP-18	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-19	Local Park	Provision of land for a local park	Yes	Melton City Council	S
			Construction and embellishment of local park	No	Developer works	S
Open Space	LP-20	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-21	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-22	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-23	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-24	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-25	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-26	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Open Space	LP-27	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-28	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-29	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-30	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-31	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-32	Local Park	Provision of land for a local park	Yes	Melton City Council	S
			Construction and embellishment of local park	No	Developer works	S
Open Space	LP-33	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-34	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-35	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-36	Local Park	Provision of land for a local park	Yes	Melton City Council	M
			Construction and embellishment of local park	No	Developer works	M

PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
Open Space	LP-37	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-38	Local Park	Provision of land for a local park	Yes	Melton City Council	S
			Construction and embellishment of local park	No	Developer works	S
Open Space	LP-39	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LP-40	Local Park	Provision of land for a local park	Yes	Melton City Council	L
			Construction and embellishment of local park	No	Developer works	L
Open Space	LOS-01	Linear Open Space - Power Easement (Plumpton)	Construction of a shared path, landscape and embellishment of linear open space.	No	Developer works	S-M
Open Space	LOS-02	Linear Open Space - Gas Easement (Plumpton)	Construction of a shared path, landscape and embellishment of linear open space.	No	Developer works	S-M
Open Space	LOS-03	Linear Open Space - Beattys Road Reserve	Construction of a shared path, landscape, embellishment of linear open space (also includes partial road functions) within the full extent of Beattys Road Reserve, as per Appendix G.	No	Developer works	M
Open Space	LOS-04	Linear Open Space - Power easement (Kororoit)	Construction of a shared path, landscape and embellishment of linear open space.	No	Developer works	S-M
Open Space	LOS-05	Linear Open Space - Gas easement (Kororoit)	Construction of a shared path, landscape and embellishment of linear open space.	No	Developer works	M-L
Open Space	-	Melton Highway Shared Path	Construction of a 2-way bike path within the south side of the existing Melton Highway Road Reservation.	No	Developer works	S-M
<b>EDUCATION PROJECTS</b>						
School	GS-01	Government Primary School	Land and construction of a government primary school in the Kororoit East Local Convenience Hub.	No	Department of Education and Training	S
School	GS-02	Government P-12 School	Land and construction of a government P-12 school in the Plumpton Major Town Centre Community Hub.	No	Department of Education and Training	M

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PROJECT CATEGORY	PIP PROJECT ID	TITLE	PROJECT DESCRIPTION	INCLUDED IN ICP	LEAD AGENCY	STAGING
School	GS-03	Government Primary School	Land and construction of a government primary school in the Plumpton Local Convenience Centre Community Hub.	No	Department of Education and Training	L
School	GS-04	Government Primary School	Land and construction of a government primary school in the Kororoit Local Town Centre Hub.	No	Department of Education and Training	M
School	GS-05	Government Primary School	Land and construction of a government primary school in the Plumpton Local Town Centre Community Hub.	No	Department of Education and Training	L
School	GS-06	Government Secondary School	Land and construction of a government secondary school in the Deanside Local Town Centre Hub.	No	Department of Education and Training	L
School	GS-07	Government Primary School	Land and construction of a government primary school in the Deanside Local Town Centre Hub.	No	Department of Education and Training	L
School	NGS-01	Non-Government Secondary School	Land and construction of a potential non-government secondary school in the Plumpton North Community Hub.	No	Non-government provider	L
School	NGS-02	Non-Government Primary School	Land and construction of a potential non-government primary school in the Plumpton Local Town Centre Community Hub.	No	Non-government provider	L
School	NGS-03	Non-Government Primary School	Land and construction of a potential non-government primary school in the Kororoit Local Town Centre Hub.	No	Non-government provider	L
<b>CONSERVATION PROJECTS</b>						
Conservation	CA-01	Conservation Area 1	Nature conservation area (abutting local road including path and nature strip on both sides of the road are developer works - all other works are by future land manager)	No	Department of Environment, Land, Water and Planning & developer works	L
Conservation	CA-02	Conservation Area 2	Nature conservation area (abutting local road including path and nature strip on both sides of the road are developer works - all other works are by future land manager)	No	Department of Environment, Land, Water and Planning & developer works	L
Conservation	CA-15	Conservation Area 15 - Growling Grass Frog Conservation Area	Growling Grass Frog conservation area (abutting local road including path and nature strip on both sides of the road are developer works - all other works are by future land manager)	No	Department of Environment, Land, Water and Planning & developer works	L

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## 7.0 APPENDICES

### Appendix A Parcel-specific land use budget

Note: If there is discrepancy due to rounding of decimal points between Appendix A and any other tables in the PSP, Appendix A takes precedence.

PLUMPTON																					
PSP PARCEL ID	TOTAL AREA (HECTARES)	TRANSPORT					COMMUNITY AND EDUCATION						OPEN SPACE					OTHER	TOTAL NET DEVELOPABLE AREA (HECTARES)	NET DEVELOPABLE AREA (% OF PROPERTY)	
		ARTERIAL ROAD				OTHER TRANSPORT	EDUCATION		COMMUNITY				UNCREDITED OPEN SPACE			CREDITED OPEN SPACE					
		ARTERIAL ROAD - EXISTING ROAD RESERVE	ARTERIAL ROAD - PUBLIC ACQUISITION OVERLAY	ARTERIAL ROAD - EXISTING ROAD RESERVE (BEATTYS ROAD)	ARTERIAL ROAD - NEW / WIDENING / INTERSECTION FLARING (ICP LAND)	NON-ARTERIAL ROAD - RETAINED EXISTING ROAD RESERVE	FUTURE GOVERNMENT SCHOOL	POTENTIAL NON-GOVERNMENT SCHOOL	LOCAL COMMUNITY FACILITY (ICP LAND)	LOCAL INDOOR RECREATION (ICP LAND)	LOCAL INDOOR RECREATION	LOCAL INDOOR RECREATION (BEATTYS ROAD)	WATERWAY AND DRAINAGE RESERVE	UTILITIES EASEMENT	LOCAL SPORTS RESERVE (BEATTYS ROAD)	SERVICE OPEN SPACE (BEATTYS ROAD)	LOCAL SPORTS RESERVE (ICP LAND)	LOCAL PARK (ICP LAND)			UTILITIES SUBSTATION/FACILITY (ACQUIRED BY RELEVANT AUTHORITY)
PROPERTY																					
1	12.2655	-	11.5985	-	-	-	-	-	-	-	-	-	0.5748	-	-	-	-	-	-	0.09	0.75%
2	12.1428	-	1.5071	-	-	-	-	-	-	-	-	-	0.9862	-	-	-	-	-	0.4001	9.25	76.17%
3	12.3395	-	2.4439	-	-	-	-	-	-	-	-	-	1.5253	-	-	-	-	-	-	8.37	67.83%
4	12.1419	-	2.1692	-	-	-	-	-	-	-	-	-	1.6625	-	-	-	-	-	-	8.31	68.44%
5	12.1138	-	2.1731	-	-	-	-	-	-	-	-	-	4.3161	-	-	-	-	0.5000	-	5.12	42.30%
6	12.1247	-	0.4169	-	0.0282	-	-	-	-	-	-	-	1.0706	-	-	-	-	-	-	10.61	87.50%
7	5.5035	-	3.2085	-	-	-	-	-	-	-	-	-	1.5124	-	-	-	-	-	-	0.78	14.22%
8	15.8344	-	2.8762	-	-	-	-	-	-	-	-	-	2.9407	-	-	-	-	-	-	0.02	0.11%
9	13.7269	-	2.2954	-	0.2926	-	-	-	-	-	-	-	0.1500	-	-	-	-	-	-	10.99	80.05%
10	12.2619	-	-	-	0.4119	-	-	-	-	-	-	-	-	-	-	-	-	0.5000	-	11.35	92.56%
11 - R	8.8431	-	-	-	1.2864	-	-	-	-	-	-	-	1.9123	-	-	-	-	-	-	5.64	63.83%
11 - E	0.9843	-	-	-	0.3439	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.64	65.06%
12 - R	69.3660	-	-	-	4.0610	-	-	-	1.0112	1.8000	1.8000	-	6.9138	-	-	-	-	0.2500	-	53.53	77.17%
12 - E	41.2610	-	-	-	0.9849	-	-	-	-	-	-	-	4.1294	-	-	-	-	0.5000	-	35.65	86.39%
13	12.0891	-	-	-	0.9513	-	-	-	-	-	-	-	1.4904	-	-	-	-	-	-	9.65	79.80%

PLUMPTON

PSP PARCEL ID	TOTAL AREA (HECTARES)	TRANSPORT					COMMUNITY AND EDUCATION						OPEN SPACE					OTHER	TOTAL NET DEVELOPABLE AREA (HECTARES)	NET DEVELOPABLE AREA (% OF PROPERTY)
		ARTERIAL ROAD				OTHER TRANSPORT	EDUCATION		COMMUNITY				UNCREDITED OPEN SPACE			CREDITED OPEN SPACE		UTILITIES SUBSTATION/FACILITY (ACQUIRED BY RELEVANT AUTHORITY)		
		ARTERIAL ROAD - EXISTING ROAD RESERVE	ARTERIAL ROAD - PUBLIC ACQUISITION OVERLAY	ARTERIAL ROAD - EXISTING ROAD RESERVE (BEATTYS ROAD)	ARTERIAL ROAD - NEW / WIDENING / INTERSECTION FLARING (ICP LAND)	NON-ARTERIAL ROAD - RETAINED EXISTING ROAD RESERVE	FUTURE GOVERNMENT SCHOOL	POTENTIAL NON-GOVERNMENT SCHOOL	LOCAL COMMUNITY FACILITY (ICP LAND)	LOCAL INDOOR RECREATION (ICP LAND)	LOCAL INDOOR RECREATION	LOCAL INDOOR RECREATION (BEATTYS ROAD)	WATERWAY AND DRAINAGE RESERVE	UTILITIES EASEMENT	LOCAL SPORTS RESERVE (BEATTYS ROAD)	SERVICE OPEN SPACE (BEATTYS ROAD)	LOCAL SPORTS RESERVE (ICP LAND)			
14	12.2515	-	-	-	1.1625	-	-	-	-	-	-	1.5218	-	-	-	-	0.5000	-	9.07	74.01%
15	12.1122	-	0.0448	-	1.0755	-	-	-	-	-	-	2.6517	-	-	-	-	-	-	8.34	68.86%
16	13.6811	-	-	-	0.2261	-	-	-	-	-	-	-	-	-	-	-	1.0000	-	12.45	91.04%
16a	0.9315	-	-	-	0.1069	-	-	-	-	-	-	-	-	-	-	-	-	0.82	88.52%	
17	14.7136	-	-	-	-	-	-	-	-	-	-	2.7610	-	-	-	0.8025	-	-	11.15	75.78%
18	14.7508	-	-	-	-	-	-	-	-	-	-	1.8117	-	-	-	5.6975	-	-	7.24	49.09%
19	14.6770	-	-	-	0.1833	-	-	7.0000	-	-	-	-	-	-	-	-	-	-	7.49	51.06%
20	14.7027	-	-	-	0.0461	-	-	-	-	-	-	-	-	-	-	-	1.0000	-	13.66	92.89%
21	16.3812	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	16.38	100.00%
22	3.0860	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	3.09	100.00%
23	15.4299	-	-	-	-	-	-	-	-	-	-	0.0550	1.5669	-	-	-	1.0000	-	12.81	83.01%
24	12.6082	-	-	-	0.2317	-	-	-	-	-	-	-	-	-	-	2.1843	-	-	10.19	80.84%
25	14.1564	-	-	-	-	-	-	-	-	-	-	-	4.6495	-	-	3.1065	-	-	6.40	45.21%
26	2.8182	-	-	-	-	-	-	-	-	-	-	-	0.7442	-	-	-	-	-	2.07	73.59%
27	62.8242	-	-	-	-	-	-	-	-	-	-	8.0681	5.2737	-	-	-	1.0000	-	48.48	77.17%
28	62.7011	-	-	-	0.1812	-	0.9627	-	1.2000	-	-	1.5016	2.4943	-	-	0.7540	1.0000	-	54.61	87.09%
29	20.5388	-	-	-	1.1251	-	4.3551	-	-	-	-	-	-	-	-	4.2148	-	-	10.84	52.80%
30	20.3686	-	-	-	1.1837	-	4.0123	-	-	-	-	-	-	-	-	4.4341	-	-	10.74	52.72%
31	18.9209	-	-	-	0.6414	-	0.6699	-	-	-	-	5.6310	-	-	-	0.5972	-	-	11.38	60.15%
32	22.4668	-	-	-	3.6753	-	-	-	-	-	-	0.5260	-	-	-	-	0.8000	-	17.47	77.74%

**PLUMPTON**

PSP PARCEL ID	TOTAL AREA (HECTARES)	TRANSPORT					COMMUNITY AND EDUCATION					OPEN SPACE					OTHER	TOTAL NET DEVELOPABLE AREA (HECTARES)	NET DEVELOPABLE AREA (% OF PROPERTY)		
		ARTERIAL ROAD				OTHER TRANSPORT	EDUCATION		COMMUNITY			UNCREDITED OPEN SPACE			CREDITED OPEN SPACE		UTILITIES SUBSTATION/FACILITY (ACQUIRED BY RELEVANT AUTHORITY)				
		ARTERIAL ROAD - EXISTING ROAD RESERVE	ARTERIAL ROAD - PUBLIC ACQUISITION OVERLAY	ARTERIAL ROAD - EXISTING ROAD RESERVE (BEATTYS ROAD)	ARTERIAL ROAD - NEW / WIDENING / INTERSECTION FLARING (ICP LAND)	NON-ARTERIAL ROAD - RETAINED EXISTING ROAD RESERVE	FUTURE GOVERNMENT SCHOOL	POTENTIAL NON-GOVERNMENT SCHOOL	LOCAL COMMUNITY FACILITY (ICP LAND)	LOCAL INDOOR RECREATION (ICP LAND)	LOCAL INDOOR RECREATION	LOCAL INDOOR RECREATION (BEATTYS ROAD)	WATERWAY AND DRAINAGE RESERVE	UTILITIES EASEMENT	LOCAL SPORTS RESERVE (BEATTYS ROAD)	SERVICE OPEN SPACE (BEATTYS ROAD)				LOCAL SPORTS RESERVE (ICP LAND)	LOCAL PARK (ICP LAND)
33	5.6808	-	-	-	1.7189	-	-	-	-	-	-	-	-	-	-	-	-	-	-	3.96	69.74%
34	14.8301	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	1.0000	-	13.83	93.26%
35	12.0016	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	5.0703	-	-	6.93	57.75%
36	6.0323	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2.4904	-	-	3.54	58.72%
37	6.0325	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2.4393	-	-	3.59	59.56%
38	12.2795	-	-	-	-	-	3.5000	2.6000	.8000	-	-	-	0.5411	-	-	-	-	-	-	4.84	39.40%
39 - R	13.7437	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	13.74	100.00%
39 - E	0.6888	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.69	100.00%
40	10.2857	-	5.3500	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	4.94	47.99%
41	14.1032	-	6.2035	-	0.4058	-	-	-	-	-	-	-	-	-	-	-	-	-	-	7.49	53.14%
42	14.4566	-	-	-	-	-	-	-	-	-	-	-	0.9490	-	-	-	-	1.0000	-	12.51	86.52%
43	14.4133	-	-	-	0.7871	-	-	-	-	-	-	-	4.9164	-	-	-	-	-	-	8.71	60.43%
44	11.9979	-	-	-	0.5965	-	-	-	-	-	-	-	1.1398	-	-	-	-	-	-	10.26	85.53%
45	11.9990	-	-	-	0.3270	-	-	-	-	-	-	-	1.2799	-	-	-	-	0.5000	-	9.89	82.44%
46	11.9993	-	-	-	0.5252	-	-	-	-	-	-	-	3.3807	-	-	-	-	-	-	8.09	67.45%
47	15.9265	-	-	-	0.5960	-	-	-	-	-	-	-	1.9527	-	-	-	-	0.9456	-	12.43	78.06%
48	93.1102	-	-	-	5.6982	-	3.5000	-	0.8000	-	-	-	2.2869	-	-	-	6.5000	1.0000	-	73.33	78.75%
49	42.0637	-	-	-	1.2453	-	-	-	-	-	-	-	9.3095	2.1259	-	-	-	0.5000	-	28.88	68.66%
50	0.0804	-	-	-	-	-	-	-	-	-	-	-	-	0.0804	-	-	-	-	-	0.00	0.00%
51	0.0561	-	-	-	-	-	-	-	-	-	-	-	-	0.0561	-	-	-	-	-	0.00	0.00%

**PLUMPTON**

PSP PARCEL ID	TOTAL AREA (HECTARES)	TRANSPORT					COMMUNITY AND EDUCATION					OPEN SPACE					OTHER	TOTAL NET DEVELOPABLE AREA (HECTARES)	NET DEVELOPABLE AREA (% OF PROPERTY)		
		ARTERIAL ROAD				OTHER TRANSPORT	EDUCATION		COMMUNITY			UNCREDITED OPEN SPACE			CREDITED OPEN SPACE						
		ARTERIAL ROAD - EXISTING ROAD RESERVE	ARTERIAL ROAD - PUBLIC ACQUISITION OVERLAY	ARTERIAL ROAD - EXISTING ROAD RESERVE (BEATTYS ROAD)	ARTERIAL ROAD - NEW / WIDENING / INTERSECTION FLARING (ICP LAND)	NON-ARTERIAL ROAD - RETAINED EXISTING ROAD RESERVE	FUTURE GOVERNMENT SCHOOL	POTENTIAL NON-GOVERNMENT SCHOOL	LOCAL COMMUNITY FACILITY (ICP LAND)	LOCAL INDOOR RECREATION (ICP LAND)	LOCAL INDOOR RECREATION	LOCAL INDOOR RECREATION (BEATTYS ROAD)	WATERWAY AND DRAINAGE RESERVE	UTILITIES EASEMENT	LOCAL SPORTS RESERVE (BEATTYS ROAD)	SERVICE OPEN SPACE (BEATTYS ROAD)	LOCAL SPORTS RESERVE (ICP LAND)			LOCAL PARK (ICP LAND)	UTILITIES SUBSTATION/FACILITY (ACQUIRED BY RELEVANT AUTHORITY)
52	0.2092	-	-	-	0.0141	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.20	93.26%
53	11.9839	-	-	-	0.0284	-	-	-	-	-	-	-	1.9699	-	-	-	-	-	-	9.99	83.33%
54	11.9998	-	-	-	-	-	-	-	-	-	-	-	2.1271	-	-	-	1.0000	-	8.87	73.94%	
55	20.2692	-	-	-	1.5382	-	-	-	-	-	-	-	3.6645	-	-	-	1.0000	-	14.07	69.40%	
<b>SUB-TOTAL</b>	<b>979.3621</b>	<b>0.0000</b>	<b>50.2872</b>	<b>0.0000</b>	<b>31.6797</b>	<b>0.0000</b>	<b>17.0001</b>	<b>9.6000</b>	<b>3.8112</b>	<b>1.8000</b>	<b>1.8000</b>	<b>0.0000</b>	<b>79.4683</b>	<b>24.7524</b>	<b>0.0000</b>	<b>0.0000</b>	<b>38.2908</b>	<b>14.9957</b>	<b>0.4001</b>	<b>705.4766</b>	<b>72.03%</b>
<b>ROAD RESERVE</b>																					
R1 (Plumpton Road)	6.5786	0.5692	0.0747	-	-	5.5393	-	-	-	-	-	-	0.3954	-	-	-	-	-	-	0.00	0.00%
R2 (Tarleton Road)	1.2677	0.8363	0.3000	-	-	-	-	-	-	-	-	-	0.1314	-	-	-	-	-	-	0.00	0.00%
R3 (Beattys Rd)	24.4487	-	1.5464	0.5568	-	4.8052	-	-	-	-	0.4297	0.9203	0.8184	0.7085	14.6634	-	-	-	-	0.00	0.00%
R4 (Saric Ct)	1.3567	0.1159	-	-	-	1.2408	-	-	-	-	-	-	-	-	-	-	-	-	-	0.00	0.00%
R5 (Taylors Rd)	3.0736	2.8420	0.1058	-	-	-	-	-	-	-	-	0.1258	-	-	-	-	-	-	-	0.00	0.00%
<b>SUB-TOTAL</b>	<b>36.7254</b>	<b>4.3635</b>	<b>2.0269</b>	<b>0.5568</b>	<b>0.0000</b>	<b>11.5853</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.4297</b>	<b>1.5729</b>	<b>0.8184</b>	<b>0.7085</b>	<b>14.6634</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.00%</b>
<b>TOTALS PSP 1078</b>	<b>1016.0875</b>	<b>4.3635</b>	<b>52.3141</b>	<b>0.5568</b>	<b>31.6797</b>	<b>11.5853</b>	<b>17.0001</b>	<b>9.6000</b>	<b>3.8112</b>	<b>1.8000</b>	<b>1.8000</b>	<b>0.4297</b>	<b>81.0412</b>	<b>25.5708</b>	<b>0.7085</b>	<b>14.6634</b>	<b>38.2908</b>	<b>14.9957</b>	<b>0.4001</b>	<b>705.4766</b>	<b>69.43%</b>

**Appendix B** Plumpton Major Town Centre design principles

**ATTRACTING INVESTMENT AND SUPPORTING THE COMMUNITY**

**Principle 1**

Create a concentration of a diverse range of uses within the town centre that are fully integrated and connected.

**Principle 2**

Create a vibrant, mixed-use town centre that acts as the focal point for business and entertainment in the region, and as an economic hub of the region.

- Include a mix of retail services such as supermarkets, specialty retailers, discount department stores, mini majors, bulky goods retailing and display-based retailing
- Provide a range of regional services to the catchment that the town centre serves such as education, health, employment, aged care and civic services
- Attract higher order government, community, civic, education and health services and investment
- Attract leisure, recreation, art and cultural uses and investment
- Provide a range of social and entertainment services such as cafes, restaurants, bars, taverns, hotels, performance venues and regional entertainment facilities
- Encourage local employment and business investment
- Encourage the establishment of serviced apartments and visitor accommodation
- Create a flexible framework which will cater for future growth, expansion and the changing trends for service delivery and investment.

**FOSTERING EMPLOYMENT**

**Principle 3**

Integrate employment and service opportunities in a business-friendly environment.

**Principle 4**

Support and promote nearby employment and industrial areas within the region.

- Cater for a substantial amount of employment opportunities including a progressive range of office premises, small-scale office/warehouse spaces and office/showroom spaces
- Support and promote nearby employment and industrial areas across Hopkins Road and within the region
- Attract a range of small, medium and large businesses to provide employment opportunities in high quality built form locations, locations with amenity, transport options and infrastructure and an active and vibrant town centre
- Foster the growth of small-scale local businesses offering services and products to the local and metropolitan markets
- Create an attractive place to conduct business for workers, clients, customers and suppliers
- Encourage the development of work-from-home and office/living opportunities in higher density housing.

**CONNECTING THE REGION**

**Principle 5**

Design the town centre to prioritise pedestrian and cyclist access and movement to and within the town centre.

**Principle 6**

Create town centres that are well served by multiple transport routes and are located at one or more major transit stop/s.

**Principle 7**

Locate town centres on or close to arterial roads/arterial intersections.

- Provide strong connections and continuous paths of travel to, from and within the town centre and industrial area to promote walking and cycling, including connections to Beattys Road and along the waterway
- Provide the town centre with direct access to the arterial road network on Hopkins Road
- Ensure the town centre is well serviced by multiple transport modes and routes
- Support the network of existing and proposed town centres within the region
- Facilitate the safe and efficient operation of bus services (including bus priority access to the proposed PPTN).

## CREATING VALUED DESTINATIONS

### Principle 8

Create a sense of place with high quality engaging urban design.

### Principle 9

Provide clear location and cultural identity for the catchment that the town centre serves.

### Principle 10

Provide connections through the town centre through a series of public spaces that are attractive environments to walk and cycle through.

### Principle 11

Focus on a public space as the centre of community life.

- Draw on the topography, waterways and historic Beattys Road alignment to develop a sense of place particular to Plumpton Town Centre
- Connect the various precincts of the town centre through an interconnected series of public spaces which encourage people to spend time in the town centre and provide opportunities for social interaction
- Ensure all public spaces are framed by a variety of uses and are active at various times of the day and night and on weekends
- Create a high quality and engaging environment with appropriate urban scale, density and intensity
- Create a central public space or 'town square' that becomes the meeting place and the 'heart' of the community
- Create a town centre that is authentic to both the local and regional communities and promotes social activities.

## DELIVERING HOUSING OPTIONS

### Principle 12

Include a range of medium and high density housing and other forms of residential uses within and around the town centre.

- Provide a range of medium and high density housing options within close proximity to the town centre to support the services and facilities on offer
- Create a fine-grained mixed use environment within the town centre which facilitates the establishment of commercial and residential outcomes
- Identify locations of high quality landscape within and surrounding the town centre where high density residential outcomes can be delivered
- Provide options for retirement living/aged care/assisted care well connected and close to the town centre, and which are sufficiently permeable to allow access between the town centre and neighbouring areas.

## RESPECTING THE ENVIRONMENT

### Principle 13

Locate the town centre in an attractive setting that respects the natural environment and history of the area.

- Ensure the town centre has a strong connection with surrounding natural features, including views to Mt Cottrell, Mt Kororoit and Mt Atkinson and that appropriate interfaces are created between the town centre and open space
- Create a regional destination which integrates the heritage of the area, including the historic Beattys Road, into the urban form and streetscapes of the town centre
- Provide regional pedestrian and cycle networks that link the natural environment to the town centre
- Integrate views to and from the existing landscape and into the design of the town centre
- Incorporate water efficiencies and WSUD principles into the design of the town centre that will contribute to the creation of green streetscapes.

**PROMOTING SUSTAINABILITY AND ADAPTABILITY**

**Principle 14**

Create a town centre which promotes regional specialisation and differentiation.

**Principle 15**

Ensure the town centre has scope for future development and expansion.

- Promote the localisation and regionalisation of services into the centre that will contribute to a reduction of travel distance to access local services and less dependence on the car
- Design the town centre to be sympathetic to its natural surrounds through integrated water management and through appropriate landscape and construction techniques
- Ensure the town centre has an inbuilt capacity for growth and change to enable adaptation and the intensification of uses as the needs of the community evolve.

The following land uses are supported within the Plumpton Major Town Centre

RETAIL	
<ul style="list-style-type: none"> <li>• Supermarkets</li> <li>• Discount department Stores</li> <li>• 'Mini-major' stores</li> <li>• Showrooms</li> <li>• Shops</li> </ul>	<ul style="list-style-type: none"> <li>• Cafes</li> <li>• Restaurants</li> <li>• Bars and clubs</li> <li>• Car parking</li> </ul>
COMMERCIAL	
<ul style="list-style-type: none"> <li>• Offices</li> <li>• Local service industry</li> <li>• Child care</li> <li>• Medical services</li> </ul>	<ul style="list-style-type: none"> <li>• Health and beauty services</li> <li>• Higher order and life-long learning services</li> <li>• Community services</li> </ul>
MIXED USE	
<ul style="list-style-type: none"> <li>• Ground floor office with upper floor residential</li> <li>• Ground floor retail with upper floor residential or office</li> </ul>	<ul style="list-style-type: none"> <li>• Mix of retail, commercial and residential areas</li> </ul>
CIVIC AND COMMUNITY FACILITIES	
<ul style="list-style-type: none"> <li>• Council facilities (library, youth services, planned activity groups)</li> <li>• Emergency services</li> </ul>	<ul style="list-style-type: none"> <li>• Health facilities</li> <li>• Place of assembly</li> </ul>
EDUCATION FACILITIES	
<ul style="list-style-type: none"> <li>• Private and independent tertiary education facilities</li> </ul>	
RESIDENTIAL	
<ul style="list-style-type: none"> <li>• Medium density residential</li> <li>• High density residential</li> <li>• Retirement living and aged care services</li> </ul>	<ul style="list-style-type: none"> <li>• SOHO (Small Office Home Office) products</li> <li>• Student accommodation</li> <li>• Serviced accommodation/apartments</li> <li>• Hotels</li> </ul>
TOWN SQUARE/PUBLIC SPACE	
<ul style="list-style-type: none"> <li>• A number of public spaces in an urban setting such as town squares, plazas, malls and urban parks.</li> </ul>	

## Appendix C Local Town Centre guidelines

PRINCIPLE	GUIDELINES
<p><b>Principle 1</b></p> <p>Provide every neighbourhood with a viable Local Town Centre as a focus of the community with a fine-grained, closely spaced distribution pattern.</p>	<ul style="list-style-type: none"> <li>• Deliver a fine-grained distribution pattern of highly accessible Local Town Centres generally on a scale of one Local Town Centre for every neighbourhood of 8,000 to 10,000 people</li> <li>• Locate Local Town Centres with a distribution pattern of around one Local Town Centre for every square mile (2.58km<sup>2</sup>) of residential development</li> <li>• Deliver a network of economically viable Local Town Centres including a supermarket and supporting competitive local shopping business, medical, leisure, recreation and community needs while allowing opportunities for local specialisation.</li> </ul>
<p><b>Principle 2</b></p> <p>Locate Local Town Centres on a connector street intersection with access to an arterial road and transit stop.</p>	<ul style="list-style-type: none"> <li>• Locate the Local Town Centre on or with close proximity to an arterial/connector intersection and ensure that the Local Town Centre is central to the residential catchment that it services while optimising opportunities for passing trade</li> <li>• Locate forms of transit stops to benefit the Local Town Centre and to offer convenience for public transport passengers</li> <li>• Other Local Town Centre locations may be considered where the location results in the Local Town Centre being central to the residential catchment that it serves and/or the location incorporates natural or cultural landscape features such as rivers and creeks, tree rows, topographic features or other heritage structures which assist in creating a sense of place.</li> </ul>
<p><b>Principle 3</b></p> <p>Locate Local Town Centres in an attractive setting so that most people live within a walkable catchment of a Local Town Centre and relate to the centre as the focus of the neighbourhood.</p>	<ul style="list-style-type: none"> <li>• Ensure that 80-90% of households are within a 1km walkable catchment of a local or higher order Town Centre</li> <li>• Locate Local Town Centres in attractive settings and incorporate natural or cultural landscape features such creeks and waterways, linear open space, pedestrian and cycle links and areas of high aesthetic value</li> <li>• The design of the Local Town Centre should respect/enhance existing views and vistas to and from the Local Town Centre location.</li> </ul>

PRINCIPLE	GUIDELINES
<p><b>Principle 4</b></p> <p>Provide a full range of local community and other facilities including a supermarket, shops, medical and recreation uses.</p>	<ul style="list-style-type: none"> <li>• Land uses should be located generally in accordance with the locations and general land use terms identified on the Local Town Centre Concept Plan</li> <li>• Promote designs which encourage a high degree of community interaction and provision of a vibrant and viable mix of retail, recreation and community facilities</li> <li>• Encourage clustering of uses in precincts such as a 'medical precinct' where similar or synergistic uses should be sited together to promote stronger trading patterns</li> <li>• Encourage smaller grain scale individual tenancies and land ownership patterns to attract participation of local business investment and encourage opportunities for greater diversity</li> <li>• Incorporate flexible floor spaces (including floor to ceiling heights) into building design to enable localised commercial uses to locate amongst the activity of the Local Town Centre</li> <li>• The Local Town Centre should generally be anchored by one full line supermarket and supported by specialty stores unless otherwise noted on the Local Town Centre Concept Plan</li> <li>• Supermarkets and other commercial or community anchors or secondary anchors within the Local Town Centre should generally be located diagonally opposite one another across the main street and/or town square to promote pedestrian desire lines that maximise movement within the public realm</li> <li>• A small access mall that address a supermarket/other 'large box uses' may be considered as part of the overall design. Such access malls may have a limited number of internalised shops. The primary access to the mall should be from the main street and/or the town square</li> <li>• Active building frontages should address the main street and town square to maximise exposure to passing trade, and promote pedestrian interaction</li> <li>• Provide retail and/or office at ground level, and office, commercial and residential above ground level in Mixed Use precincts</li> <li>• Locate childcare, medical centres and specialised accommodation (e.g. aged care/nursing home, student accommodation, and serviced apartments) within and at the edge of the Local Town Centre to contribute to the activity of the centre and so these uses are close to the services offered by the centre</li> <li>• Locate car parking areas centrally to development sites and to the rear and or side of street based retail frontages</li> <li>• Design car parking areas to accommodate flexible uses and allow for long term development opportunities</li> <li>• Provide public toilets in safe and accessible locations within the managed area of the property.</li> </ul>

PRINCIPLE	GUIDELINES
<p><b>Principle 5</b></p> <p>Focus on a public space as the centre of community life.</p>	<ul style="list-style-type: none"> <li>• Provide a public space which acts as the central meeting place within the Local Town Centre. This space may take the form of a town square, town park, public plaza space, public market place or a similar locally responsive option designed to function as the identifiable 'centre' or 'heart' with a distinctive local character for both the Local Town Centre and the broader residential catchment</li> <li>• Locate the public space in a position where the key uses of the Local Town Centre are directly focused on it to ensure that it is a dynamic and activated place</li> <li>• Design flexible and adaptable public spaces so that a range of uses can occur within them at any one time. Such uses may include people accessing daily shopping and business needs as well as social interaction, relaxation, celebrations and temporary uses (such as stalls, exhibitions and markets)</li> <li>• Design the public space so that it is well integrated with pedestrian and cycle links around and through the Local Town Centre so that it acts as a 'gateway' to the activity of the centre</li> <li>• The main public space or town square should have a minimum area of 500square metres. Smaller public spaces which are integrated within the built form design, surrounded by active frontages and facilitate high levels of pedestrian movement are also encouraged</li> <li>• Footpath widths within and around the public space as well as along the main street should be sufficient to provide for universal access as well as outdoor dining and smaller gathering spaces.</li> </ul>
<p><b>Principle 6</b></p> <p>Integrate local employment and service opportunities in a business friendly environment.</p>	<ul style="list-style-type: none"> <li>• Provide a variety of employment and business opportunities through the provision of a broad mix of land uses and commercial activities</li> <li>• Provide a range of options and locations for office based businesses</li> <li>• Provide services and facilities to support home based and smaller businesses within the Local Town Centre</li> <li>• Consider appropriate locations for small office/home office ('SOHO') housing options which maximise the access and exposure to the activity of the Local Town Centre</li> <li>• Consider using these uses to sleeve loading areas and car parks where feasible.</li> </ul>
<p><b>Principle 7</b></p> <p>Include a range of medium and high density housing and other forms of residential uses within and surrounding the Local Town Centre.</p>	<ul style="list-style-type: none"> <li>• Provide medium and high density housing in and around the Local Town Centre for passive surveillance and contributions to the life and amenity of the centre</li> <li>• Provide medium and high density housing in locations of high amenity in and around the Local Town Centre, connected to the activity of the Local Town Centre through strong pedestrian and cycle links</li> <li>• Provide a range of housing types for a cross section of the community (such as retirement living) in and around the Local Town Centre</li> <li>• Provide specialised accommodation (such as aged/nursing care, student accommodation and serviced apartments) at the edge of or adjacent to Local Town Centres with strong pedestrian and cycle links to the central activity area</li> <li>• Design the Local Town Centre to avoid potential land use conflicts between residential and commercial uses by focusing on retail operations on the main street and around the town square and locating residential uses predominantly at the edge and/or on upper levels</li> <li>• Refer to the Small Lot Housing Code for further information about housing requirements for small lots around Local Town Centres.</li> </ul>

PRINCIPLE	GUIDELINES
<p><b>Principle 8</b></p> <p>Design the Local Town Centre to be pedestrian friendly and accessible by all modes including public transport, while enabling private vehicle access.</p>	<ul style="list-style-type: none"> <li>• Use universal design principles in the design of all public spaces</li> <li>• Design the Local Town Centre to provide easy, direct and safe access for pedestrians, cyclists, public transport modes, private vehicles, service and delivery vehicles with priority given to pedestrian movement, amenity, convenience and safety</li> <li>• Provide a permeable network of streets, walkways and public spaces that provide linkages throughout the centre and designated pedestrian crossing points</li> <li>• Design the main and other streets to comply with the relevant cross sections found within the precinct structure plan</li> <li>• A speed environment of 40km/h or less should be designed for the length of the main street</li> <li>• Provide public transport infrastructure facilities in convenient locations for commuters</li> <li>• Provide bus stops in accordance with the Department of Transport Public Transport Guidelines for Land Use and Development, to the satisfaction of the Public Transport Victoria</li> <li>• Provide bicycle parking within the street network and public spaces in highly visible locations and close to key destinations</li> <li>• Design supermarket and other 'large format' buildings so they do not impede on the movement of people around the Local Town Centre</li> <li>• Locate key buildings to encourage pedestrian movement along the length of the street and through public spaces</li> <li>• Design buildings so they have a positive relationship with and interface to the public street network</li> <li>• Design car parking areas to ensure passive surveillance and public safety through adequate positioning and lighting</li> <li>• Provide dedicated pedestrian routes and areas of landscaping within off street car park areas</li> <li>• Provide on-street car parking to encourage short stay/convenience uses</li> <li>• Group and limit the number of car park access crossovers</li> <li>• Design heavy vehicle access points to limit the pedestrian/vehicle conflict. Loading and deliveries should be located to the rear and or side of street based retail frontages</li> <li>• All streets, public spaces and car parks to be lit to Australian standards and with pedestrian friendly (generally white) light. Lighting should be designed to avoid unnecessary spill to the side or above.</li> </ul>
<p><b>Principle 9</b></p> <p>Create a sense of place with high quality engaging urban design</p>	<ul style="list-style-type: none"> <li>• Design development to complement and enhance the character of the surrounding area by responding to key visual cues associated with the topography and other natural features of the Local Town Centre location and its surrounds</li> <li>• Minimise amenity and noise impacts resulting from the mix of uses by maintaining appropriate separation and transitional areas between retail and housing activities using open space, road networks and community facilities</li> <li>• Design each building to contribute to a cohesive and legible character for the Local Town Centre as a whole</li> <li>• Designate sites in prominent locations (such as at key intersections, surrounding public spaces and terminating key view lines and vistas) for significant buildings or landmark structures</li> <li>• Design corner sites, where the main street meets an intersecting and/or arterial road to:             <ul style="list-style-type: none"> <li>• Provide built form that anchors the main street to the intersecting road. This can be achieved through increased building height, scale and articulated frontages</li> <li>• Incorporate either 2 storey building or 2 storey elements (such as awnings and roof lines)</li> <li>• Provide an active ground floor frontage and active floor space component to the main street frontage; and</li> </ul> </li> <li>• Provide a consistent covered walkway or verandah for weather protection in the design of building frontages on major pedestrian routes</li> <li>• Align built form with the property boundary to define the street edge</li> <li>• Provide visually rich, interesting and well articulated street facing facades and all visible side or rear facades finished in suitable materials and colours that contribute to the character of the Local Town Centre</li> </ul>

PRINCIPLE	GUIDELINES
<p><b>Principle 9</b> <i>Continued</i></p>	<ul style="list-style-type: none"> <li>• Use materials and design elements which are compatible with the environment and landscape character of the broader precinct</li> <li>• The design and siting of supermarkets and other 'large format retail uses' should provide an appropriate response to the entire public domain. This includes but is not limited to car parking areas, predominantly routes and streets</li> <li>• Design supermarket and secondary anchors with frontages that directly address the main street and/or town square so that the use integrates with and promotes activity within the main street and public spaces/thoroughfares</li> <li>• Design supermarkets or large format retail uses with a direct frontage to the main street using clear glazing to allow view lines into the store from the street. (Planning permits for buildings and works should condition against the use of white washed or frosted glass windows, excessive window advertising and obtrusive internal shelving or 'false walls' offset from the glazing)</li> <li>• Secondary access to the supermarket from car parking areas should only be considered where it facilitates convenient trolley access and does not diminish the role of the primary access from the main street and or town square</li> <li>• Retail uses along street frontages should generally include access points at regular intervals to encourage activity along the length of the street</li> <li>• Retail and commercial buildings within the Local Town Centre should generally be built to the property line</li> <li>• Public spaces should be oriented to capture north sun and protect from prevailing winds and weather</li> <li>• Landscaping of all interface areas should be of a high standard as an important element to complement the built form design.</li> <li>• Urban art should be incorporated into the design of the public realm</li> <li>• Street furniture should be located in areas that are highly visible and close to or adjoining pedestrian desire lines/gathering spaces and designed to add visual interest to the Local Town Centre</li> <li>• Wrapping or sleeving of car parking edges with built form, to improve street interface, should be maximised</li> <li>• Car parking areas should provide for appropriate landscaping with planting of canopy trees and dedicated pedestrian thoroughfares</li> <li>• Screening of centralised waste collection points should minimise amenity impacts on adjoining areas and users of the centre</li> <li>• Where service areas are accessible from car parks, they should present a well-designed and secure facade to public areas</li> <li>• Mechanical plant and service structure roofs should be included within roof lines or otherwise hidden from view.</li> </ul>
<p><b>Principle 10</b> Promote localisation, sustainability and adaptability.</p>	<ul style="list-style-type: none"> <li>• The Local Town Centre should promote the localisation of services which will contribute to a reduction of travel distance to access local services and less dependence on private vehicles</li> <li>• The Local Town Centre should be designed to be sympathetic to its natural surrounds by:             <ul style="list-style-type: none"> <li>• Investigating the use of energy efficient design and construction methods for all buildings</li> <li>• Including Water Sensitive Urban Design principles such as integrated stormwater retention and reuse (e.g. toilet flushing and landscape irrigation)</li> <li>• Promoting safe and direct accessibility and mobility within and to and from the Local Town Centre</li> <li>• Including options for shade and shelter through a combination of landscape and built form treatments</li> <li>• Ensuring buildings are naturally ventilated to reduce the reliance on plant equipment for heating and cooling</li> <li>• Promoting passive solar orientation in the configuration and distribution of built form and public spaces</li> <li>• Grouping waste collection points to maximise opportunities for recycling and reuse</li> <li>• Promoting solar energy for water and space heating, electricity generation and internal and external lighting; and</li> <li>• Investigating other opportunities for the built form to reduce greenhouse gas emissions associated with the occupation and the ongoing use of buildings.</li> </ul> </li> <li>• Ensure the Local Town Centre and building design has an inbuilt capacity for growth and change to enable adaptation and the intensification of uses as the needs of the community evolve.</li> </ul>

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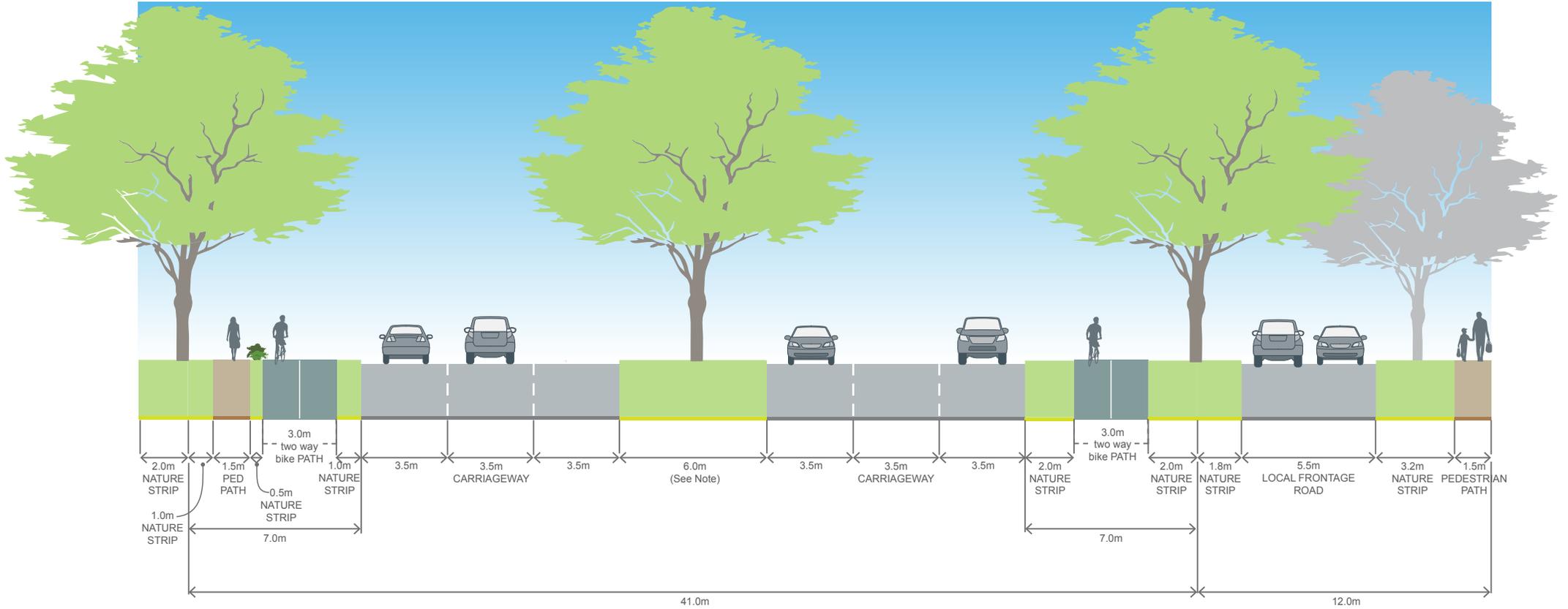
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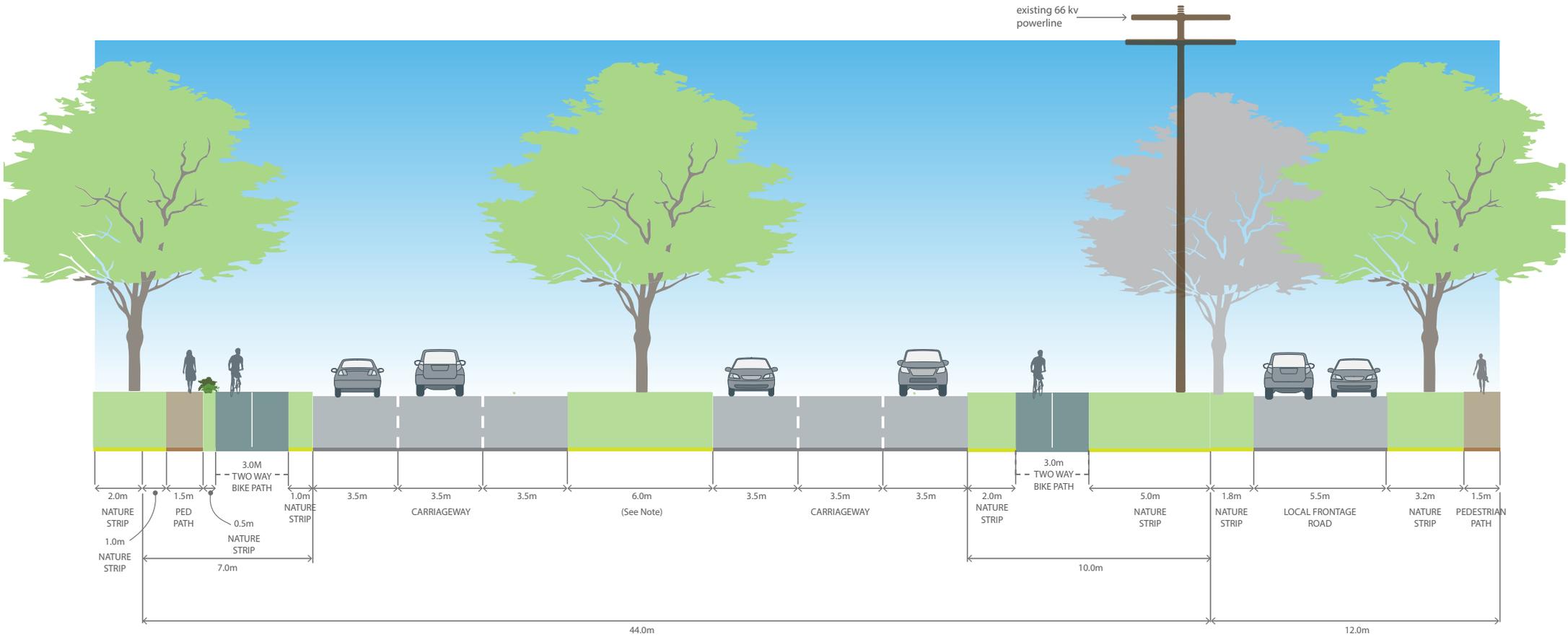
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# Road Cross Sections



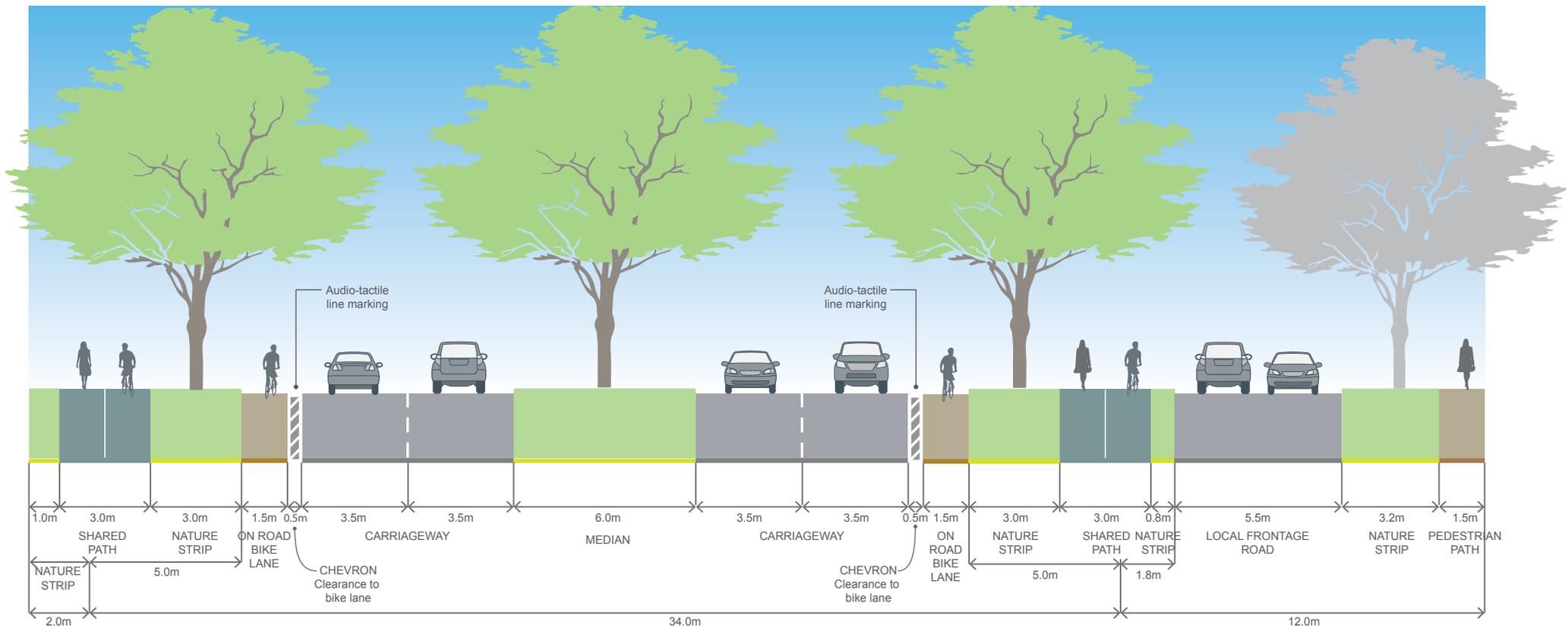
## NOTES:

- Includes typical residential interface both sides
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb (refer Engineering Design and Construction Manual for Subdivision in Growth Areas, April 2011)
- See VicRoads Tree Planting Policy. Large trees within the road reserve to be protected by safety barriers, else small tree <100mm Ø trunk at double spacing)
- Frontage road widths may vary subject to detailed design
- Include low level plantings on the 0.5m strip, to delineate between pedestrian path and bike path.



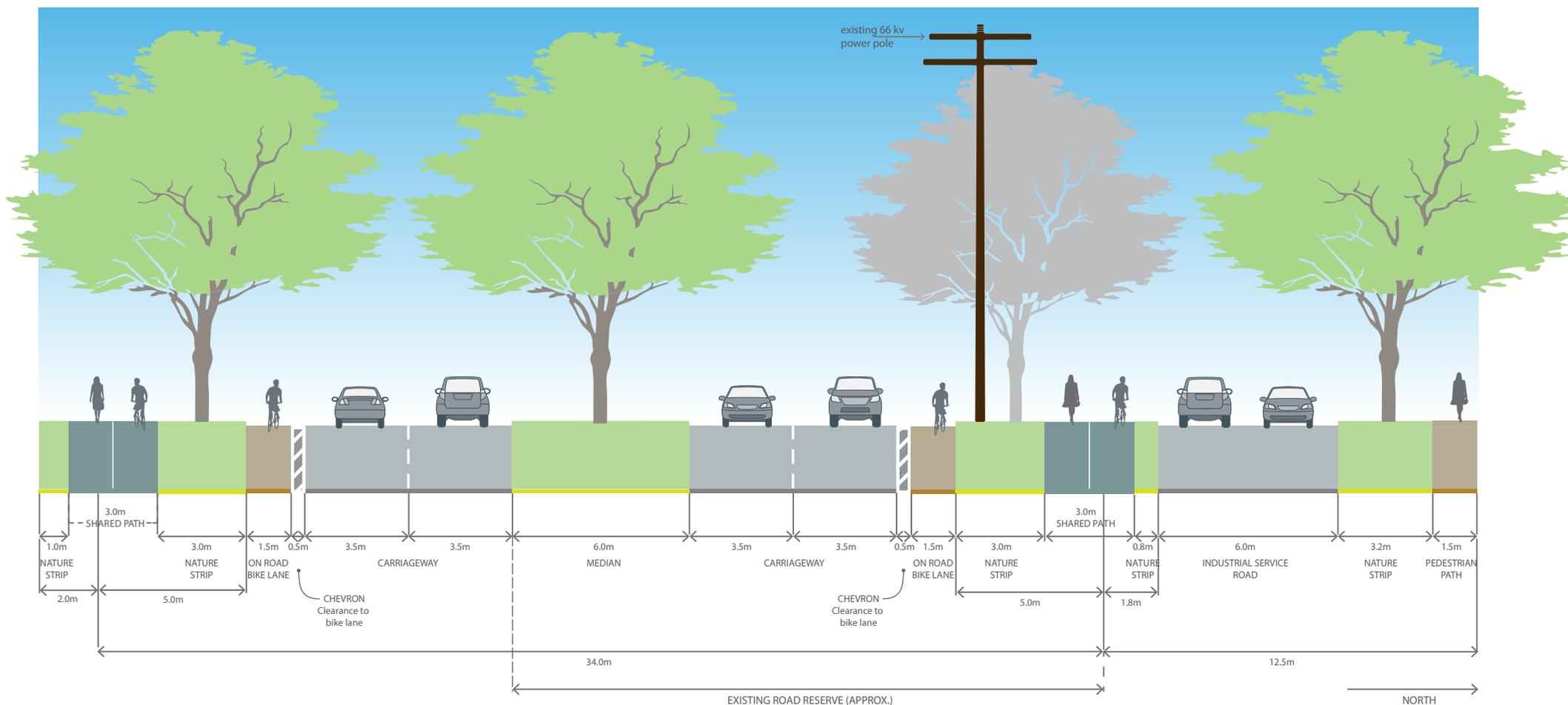
NOTES:

- Includes typical residential interface both sides
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- See VicRoads Tree Planting Policy. Large trees within the road reserve to be protected by safety barriers, else small tree <100mm ø trunk at double spacing)
- Existing power lines to be relocated where necessary
- Frontage road widths may vary subject to detailed design
- Include low level plantings on the 0.5m strip, to delineate between pedestrian path and bike path.



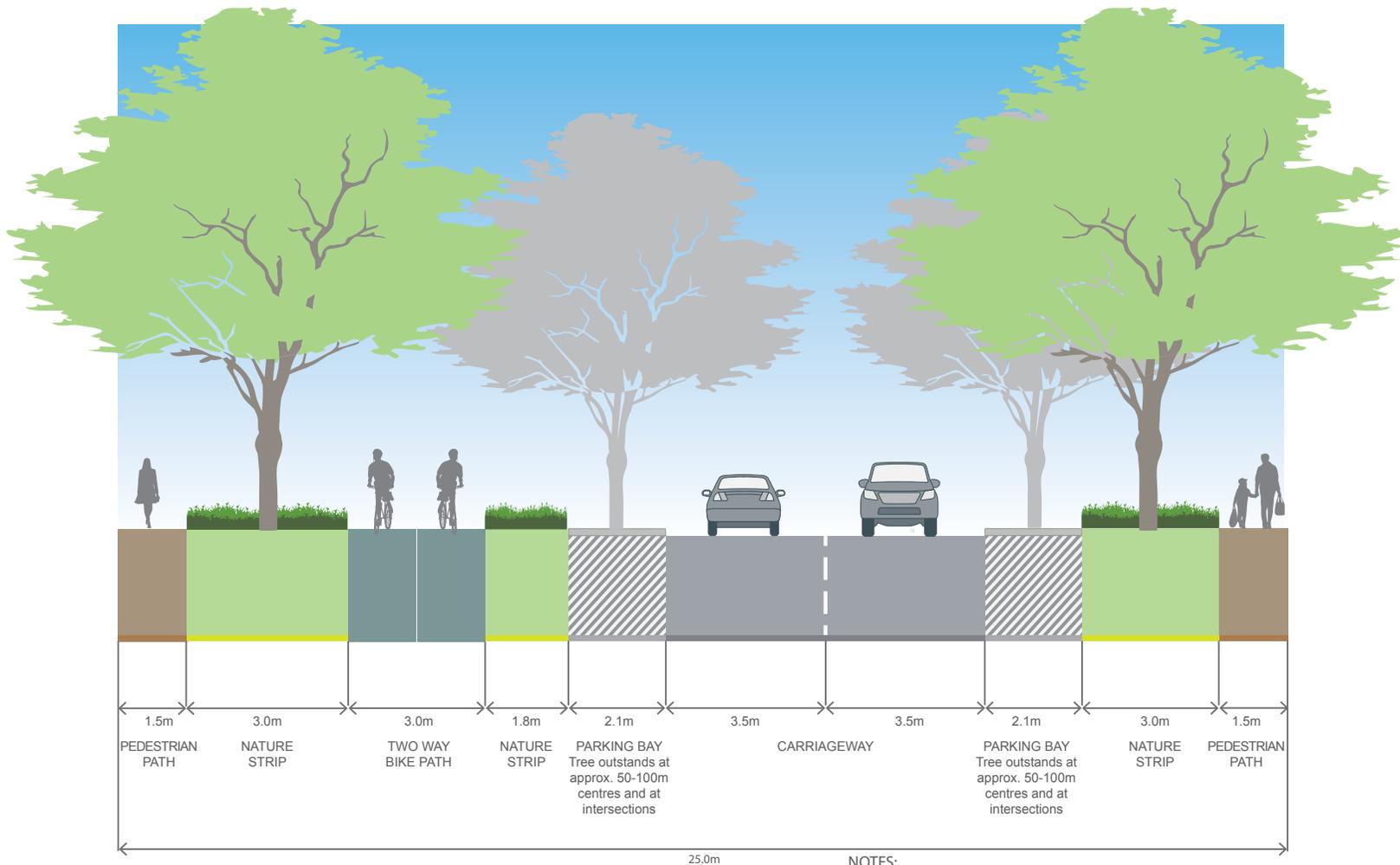
NOTES:

- Includes typical residential interface both sides
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Frontage road widths may vary subject to detailed design.



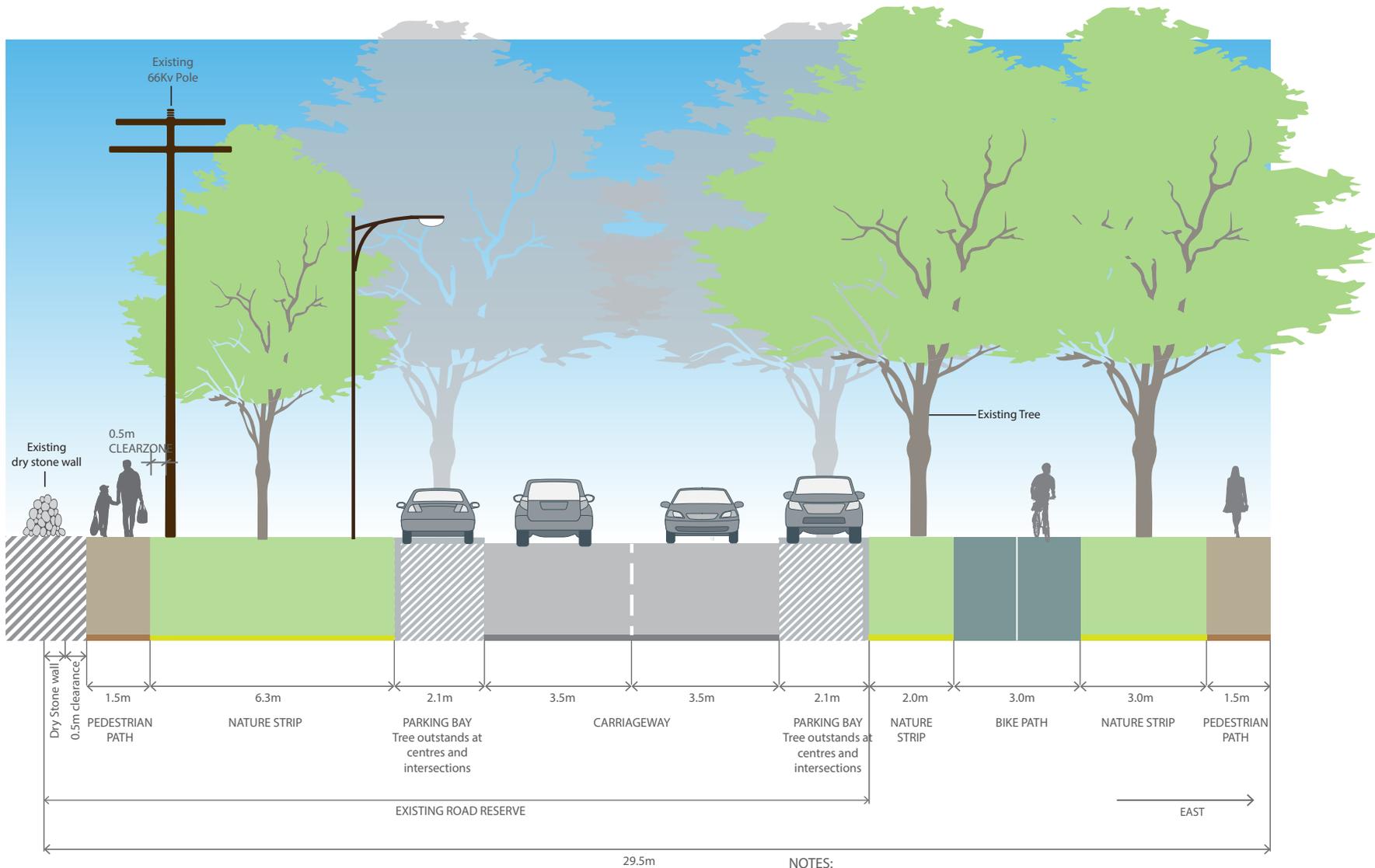
NOTES:

- Includes typical residential interface on one side
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- See VicRoads Tree Planting Policy. Large trees within the road reserve to be protected by safety barriers, else small tree <100mm ø trunk at double spacing)
- Existing power poles to be relocated where necessary
- Frontage road widths may vary subject to detailed design.



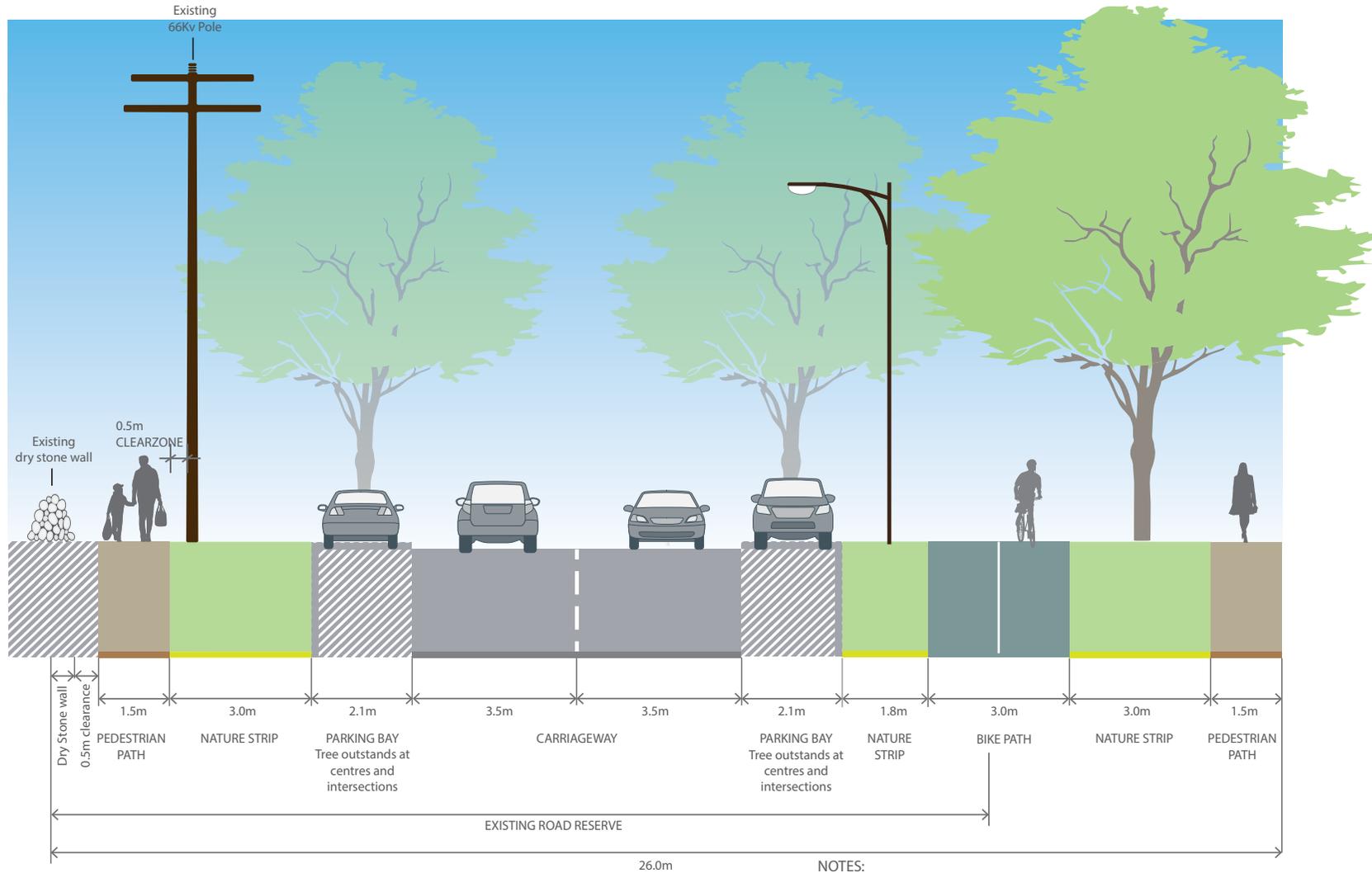
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Where roads abut school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Tree outstands must meet a maximum interval of 100m.



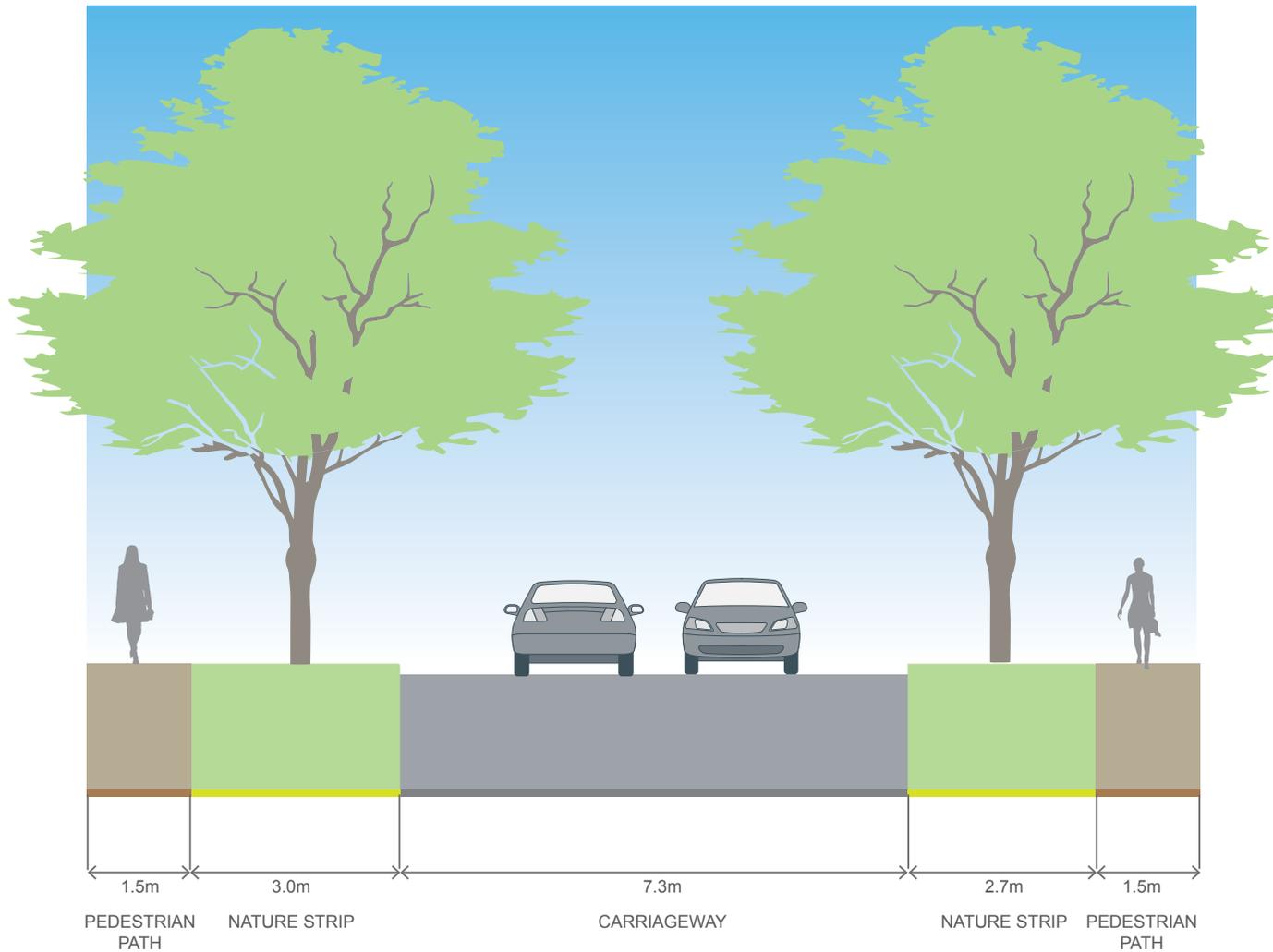
- NOTES:
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
  - Where roads abut school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement
  - Kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
  - Verge widths may be reduced where roads abut open space with the consent of the responsible authority
  - Tree outstands must meet a maximum interval of 100m.

**Section 6**  
Plumpton Road (29.5m)



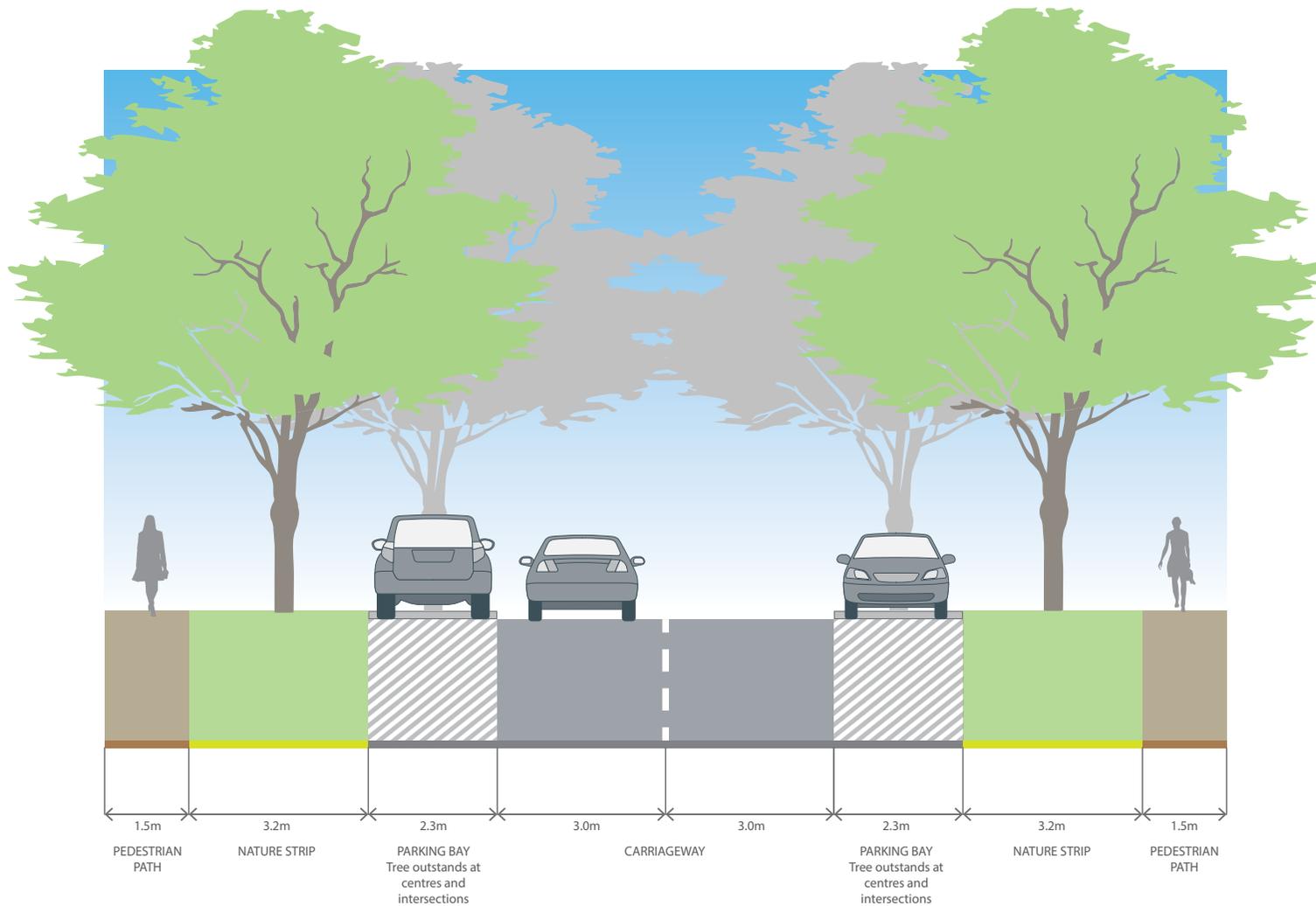
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Canopy tree planting must be incorporated into any additional pavement
- Where roads abut school drop-off zones and grassed nature strip should be replaced with pavement
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Tree outstands must meet a maximum interval of 100m.



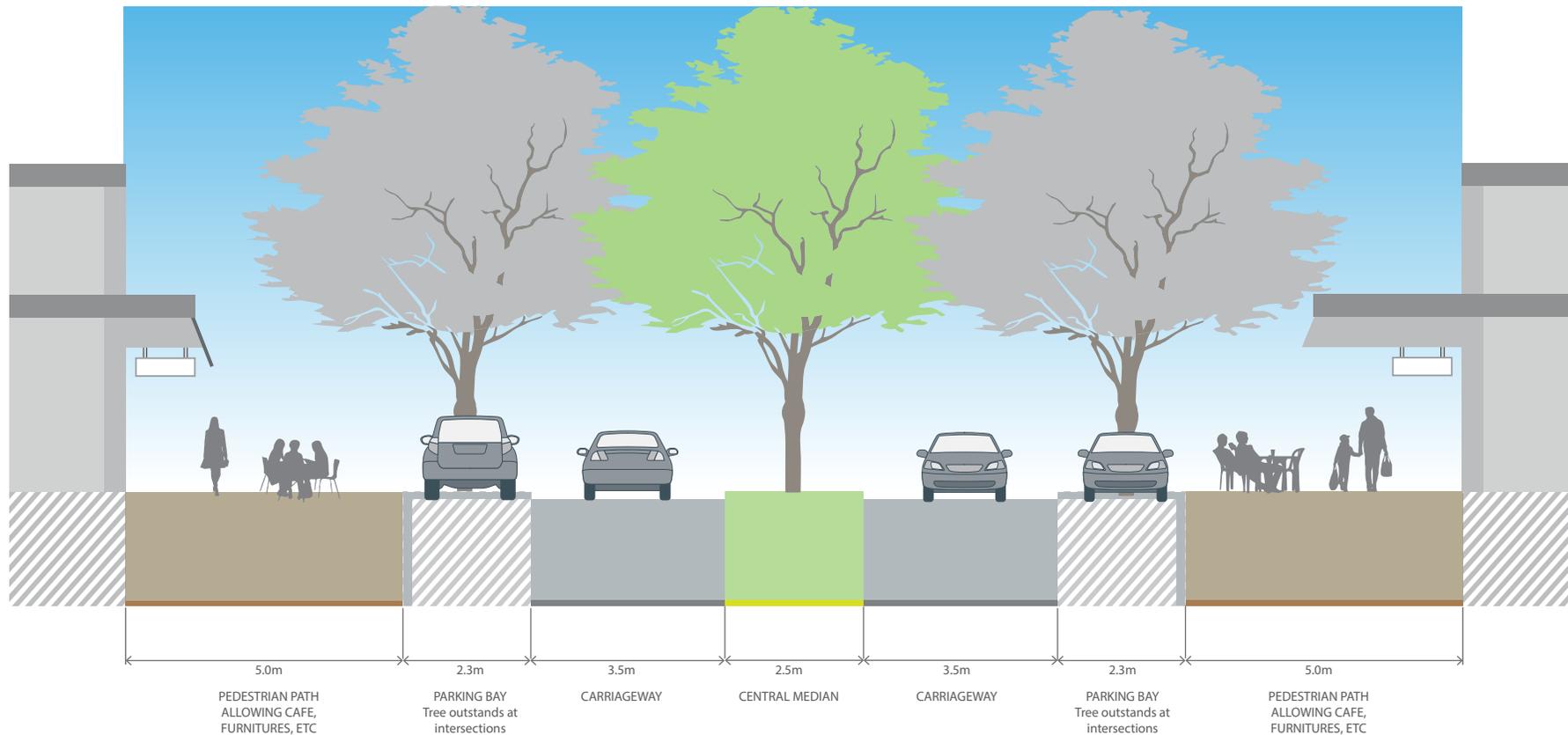
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Local access streets abutting schools are to be local access street level 2 (20m) type roads.



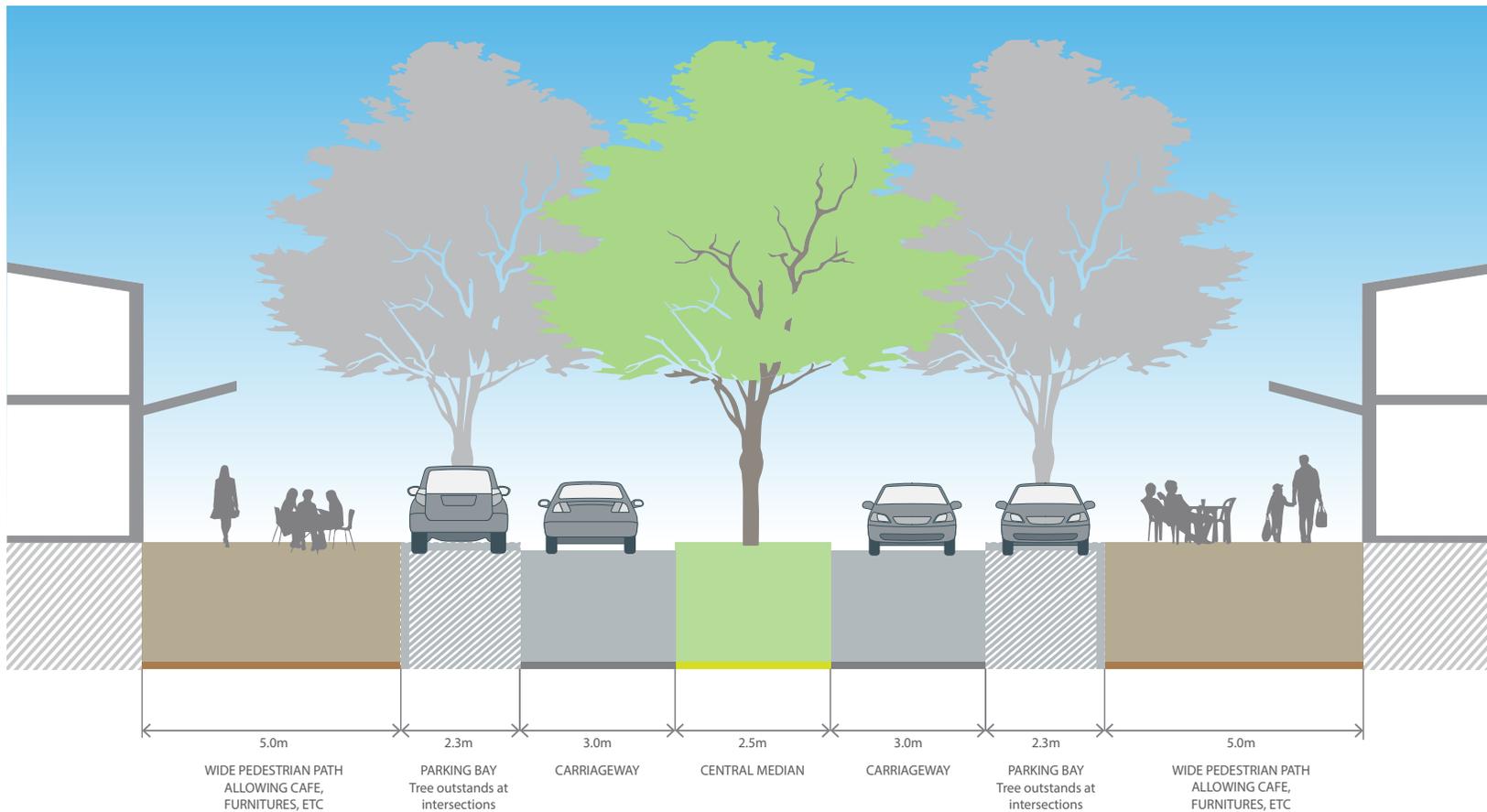
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Where roads abut school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement
- Local access streets abutting schools are to be local access street level 2 (20m) type roads
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Tree outstands must meet a maximum interval of 100m.



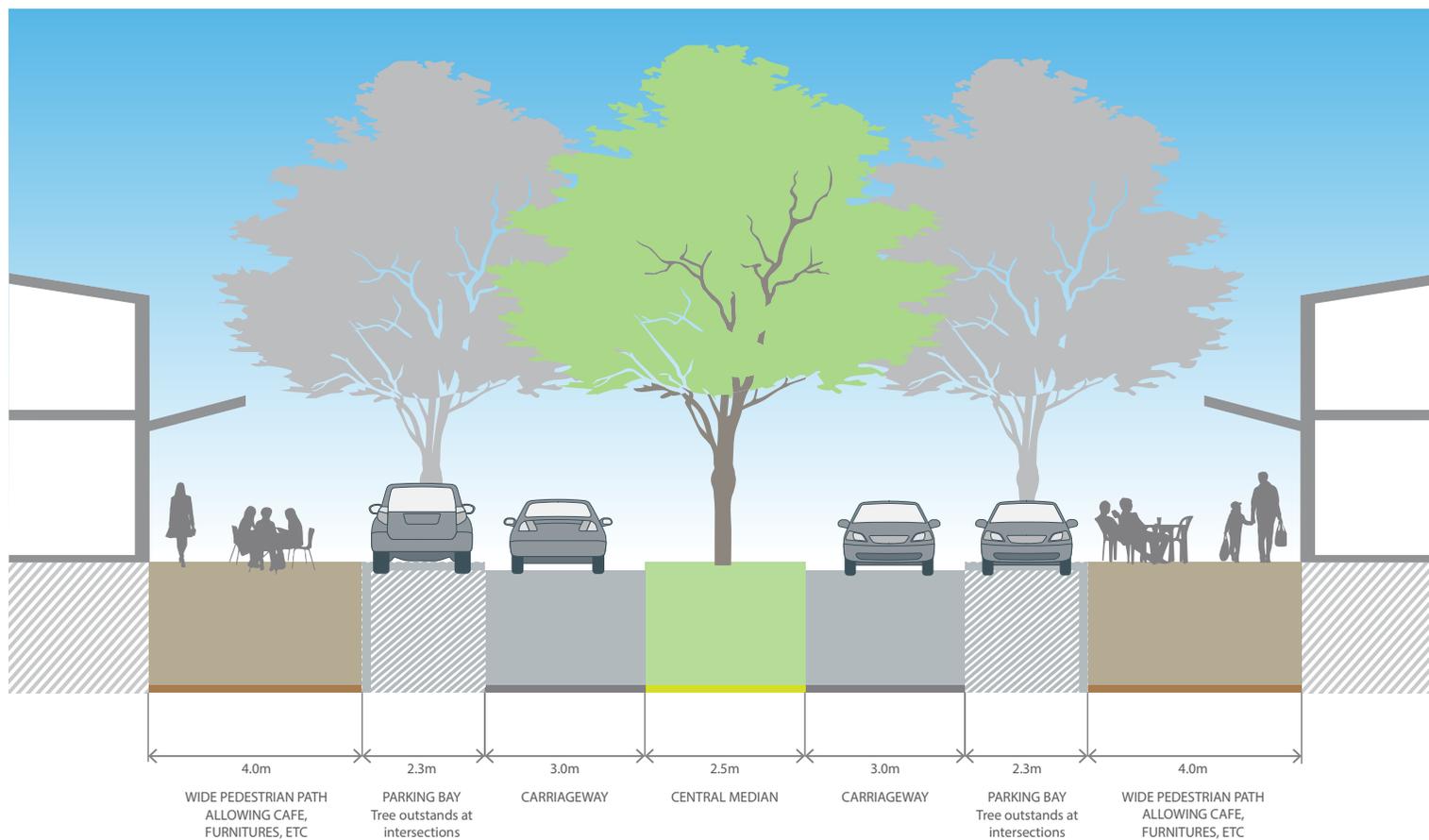
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Road to be designed with traffic calming devices, including raised pedestrian crossings and roundabouts to achieve a speed limit of 30km/h to allow safe on road cycling
- Tree outstands must meet a maximum interval of 100m.



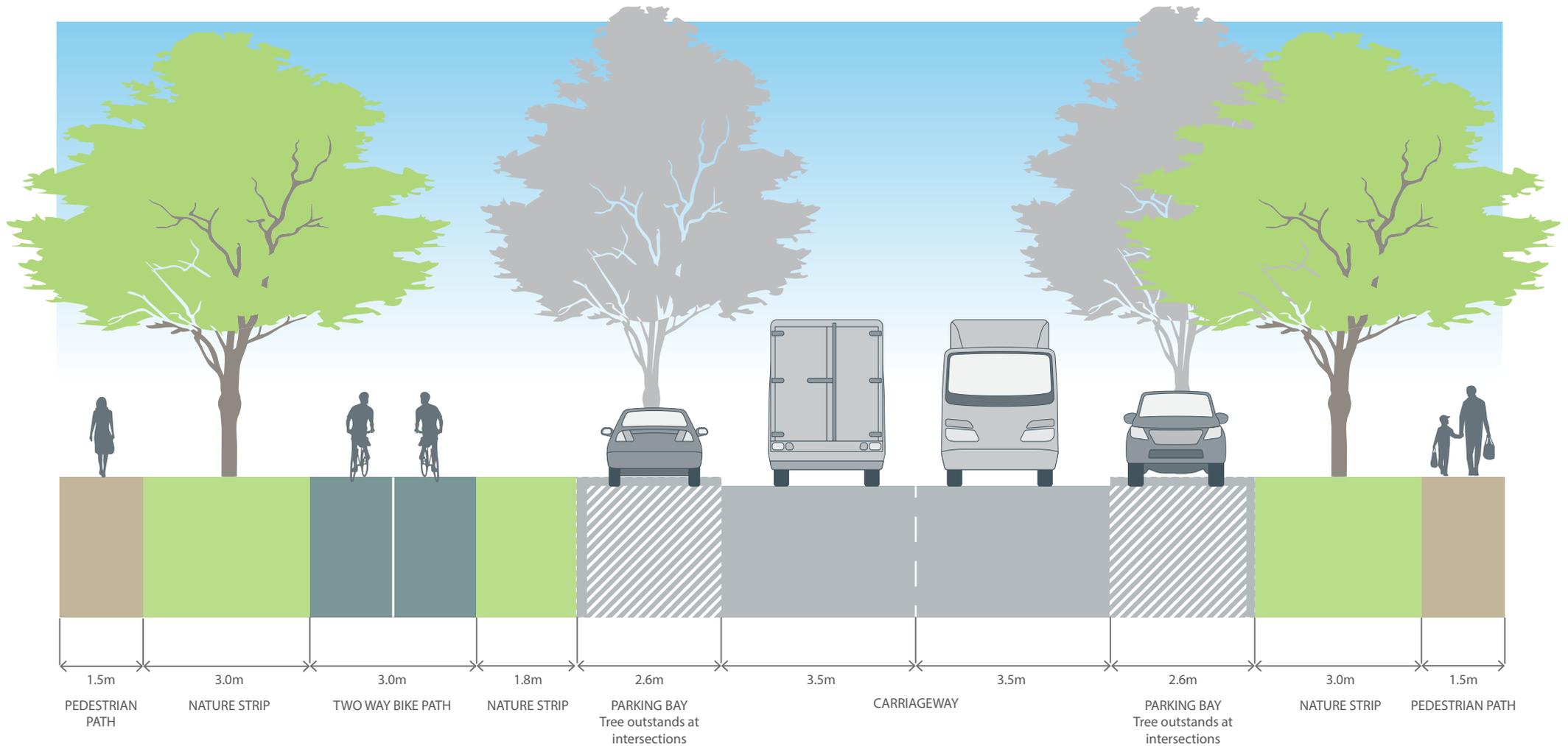
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
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- Tree outstands must meet a maximum interval of 100m.



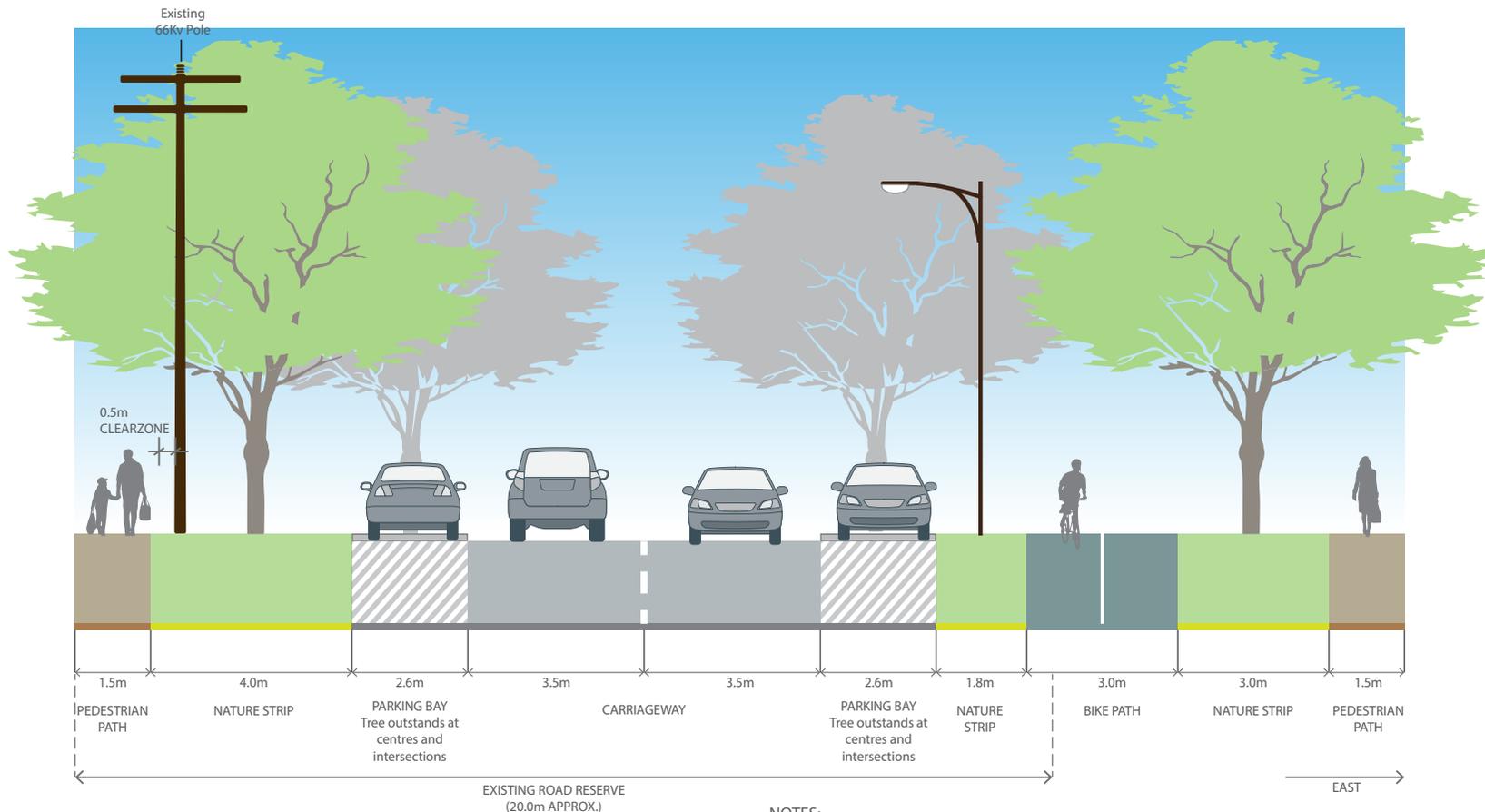
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Road to be designed with traffic calming devices, including raised pedestrian crossings and roundabouts to achieve a speed limit of 30km/h to allow safe on road cycling
- Tree outstands must meet a maximum interval of 100m.



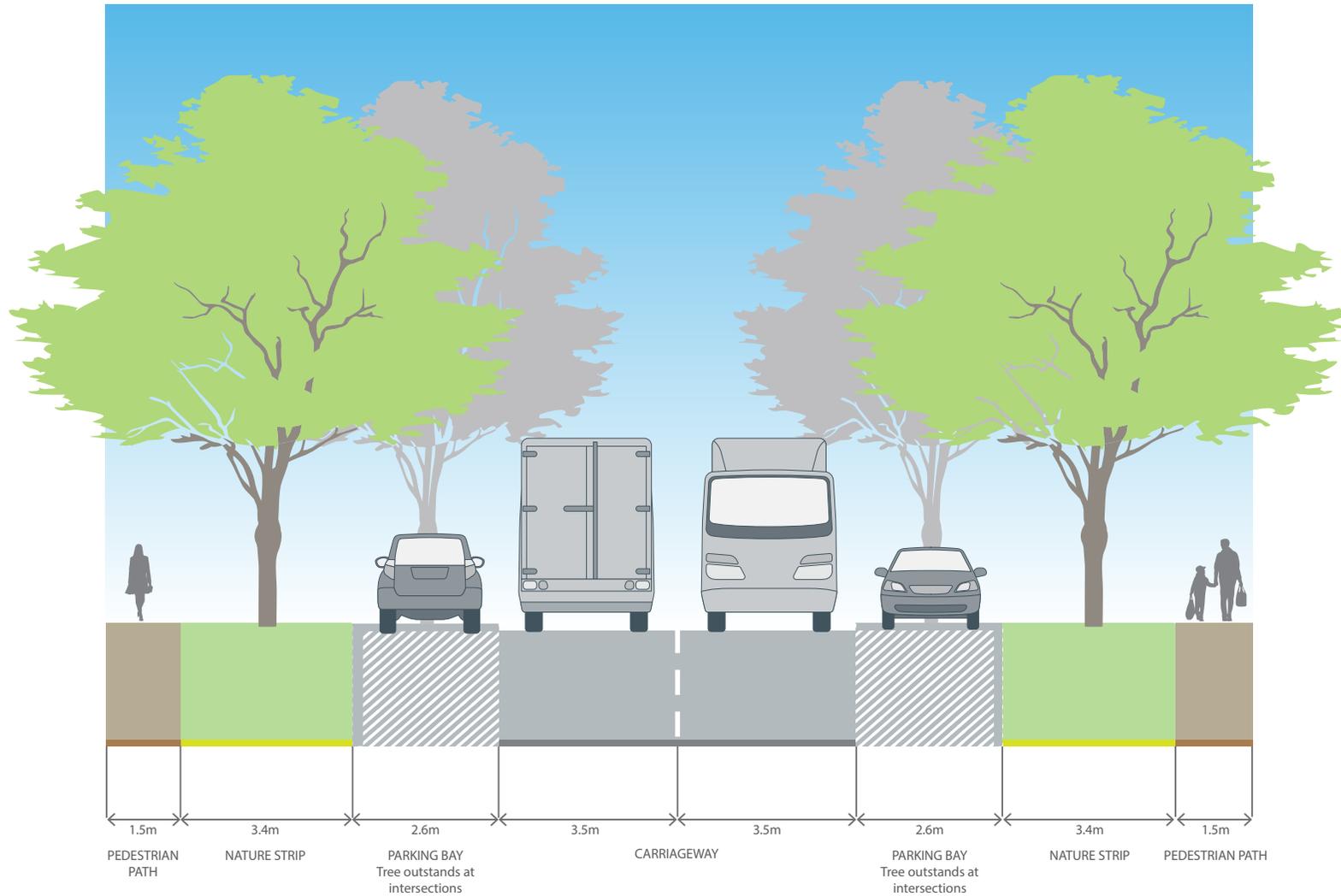
NOTES:

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- Kerbs for arterial carriageways are to be SM2 Semi-Mountable Kerb, and local frontage roads are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Where roads abut school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Tree outstands must meet a maximum interval of 100m.



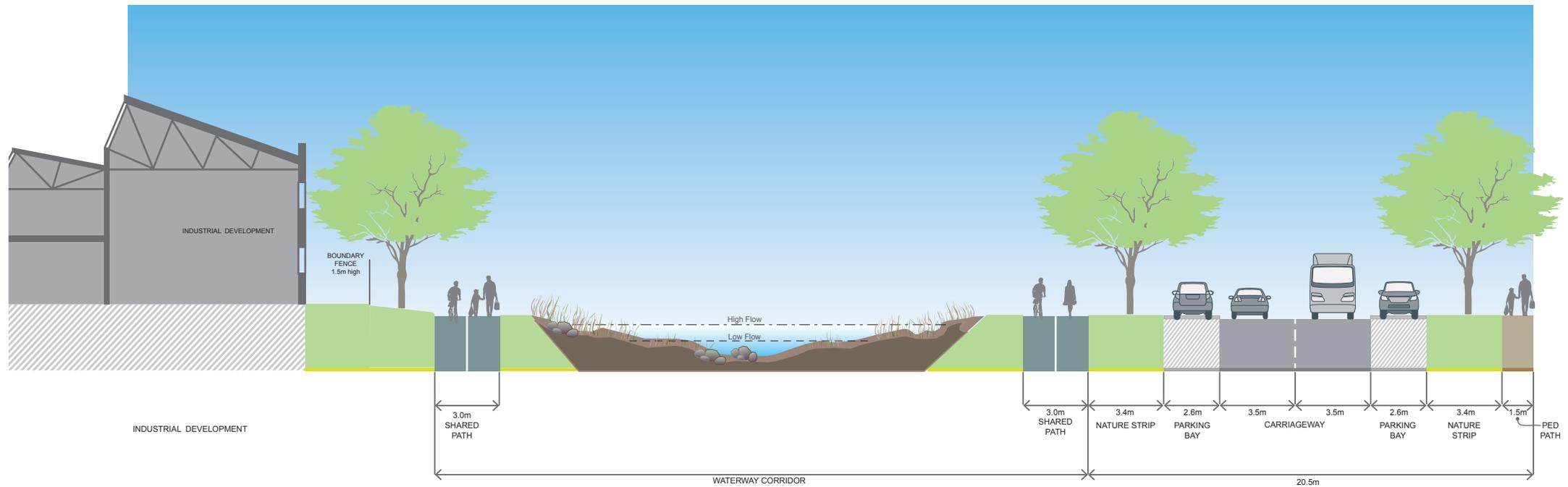
**NOTES:**

- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Where roads abut school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Tree outstands must meet a maximum interval of 100m.



NOTES:

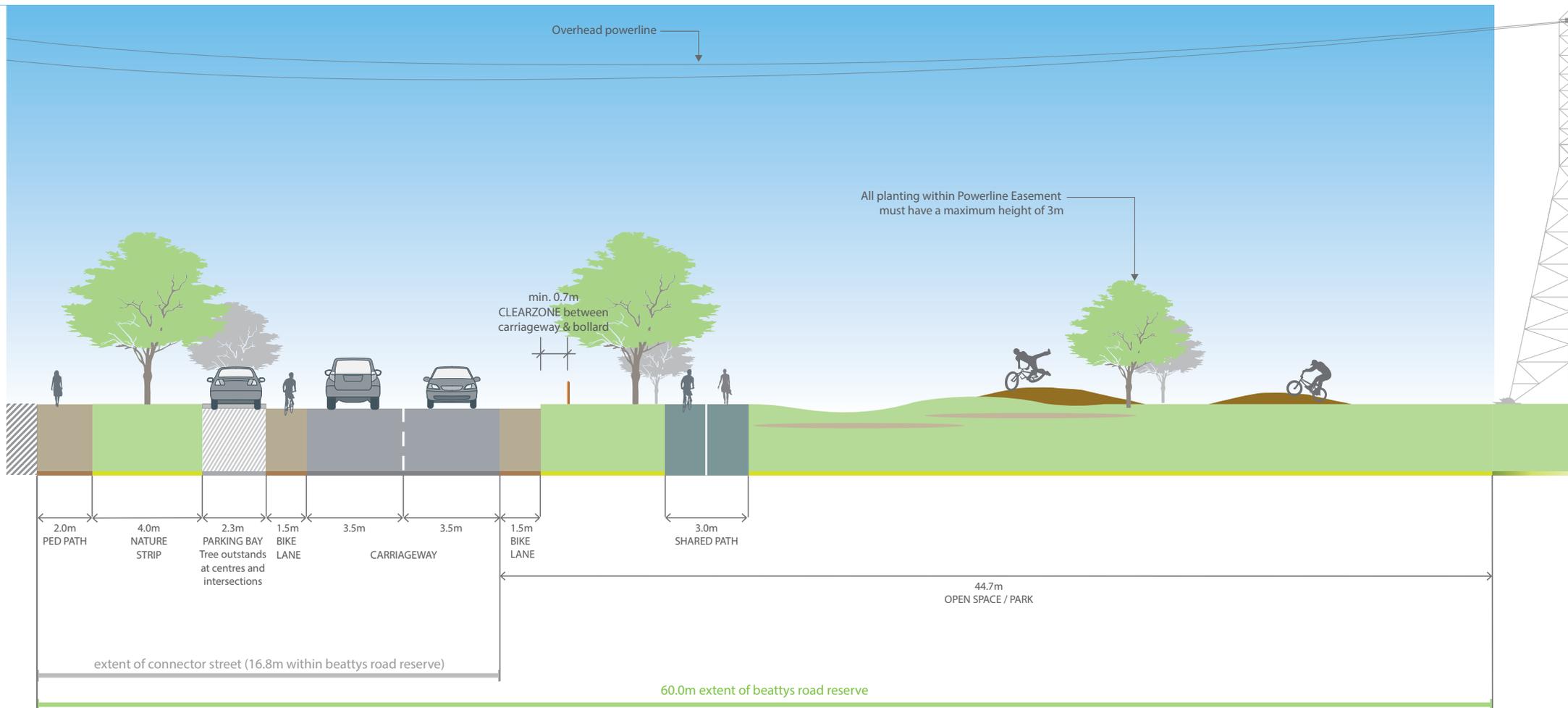
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- Where roads abut school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement
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- Tree outstands must meet a maximum interval of 100m.



NOTES:

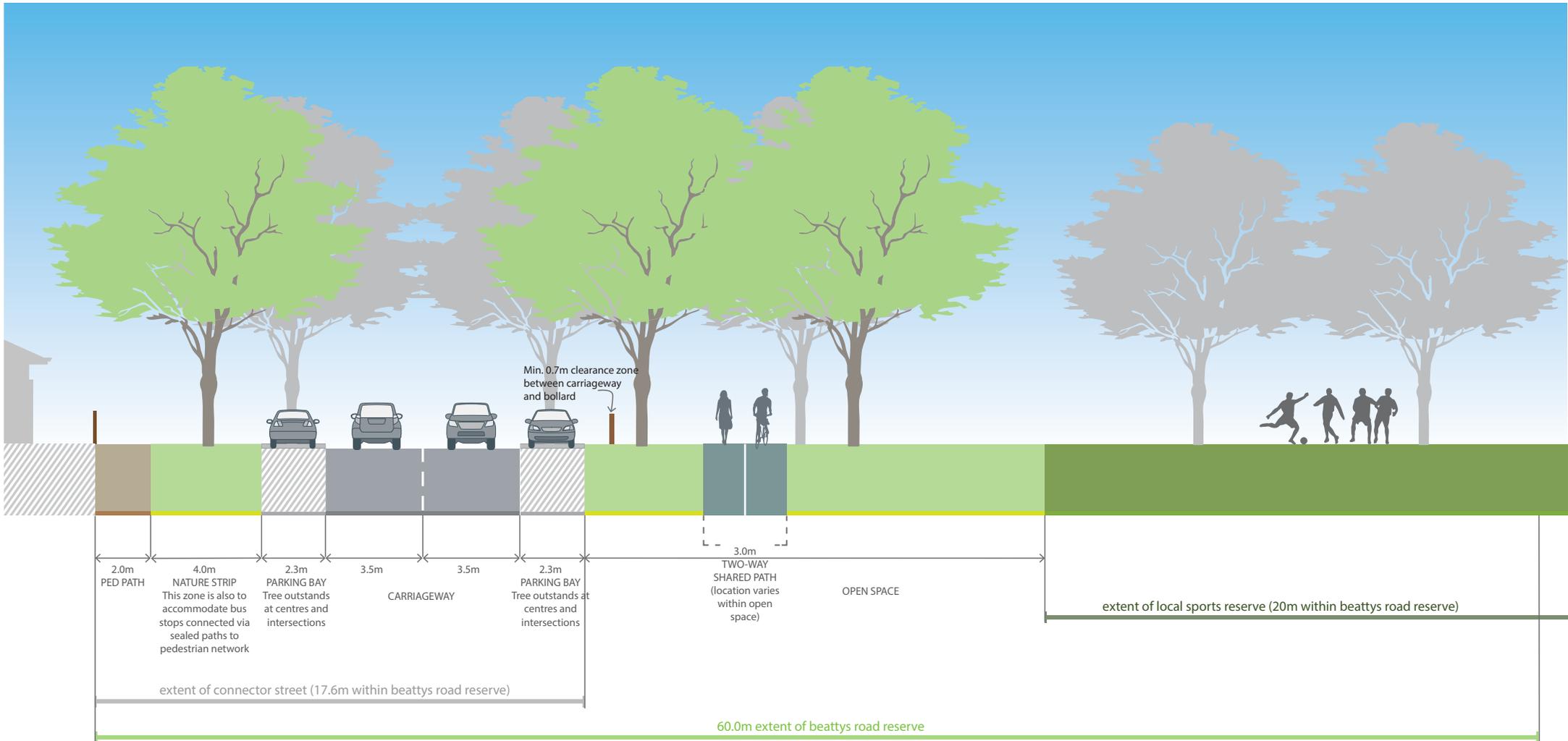
- Industrial buildings should provide attractive interface to the shared path and waterway
- Waterway widths subject to Melbourne Water approval
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Where roads abut school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority.

# Beattys Road Reserve Cross Sections



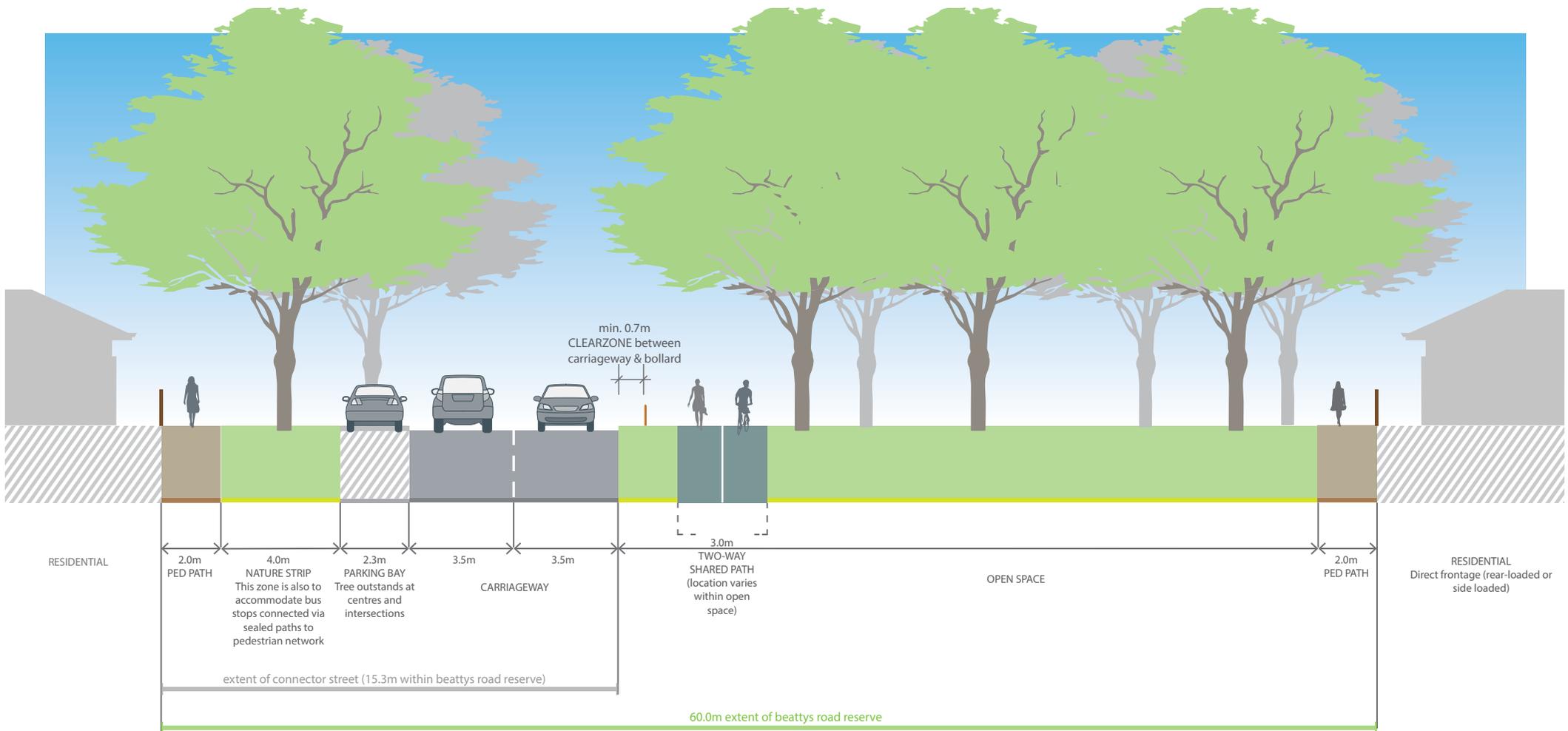
**NOTES:**

- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision. in Growth Areas
- Tree outstands must meet a maximum interval of 100m
- Mature street tree size must be in accordance with Melton City Council's landscaping policy.



**NOTES:**

- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m
- Mature street tree size must be in accordance with Melton City Council's landscaping policy.



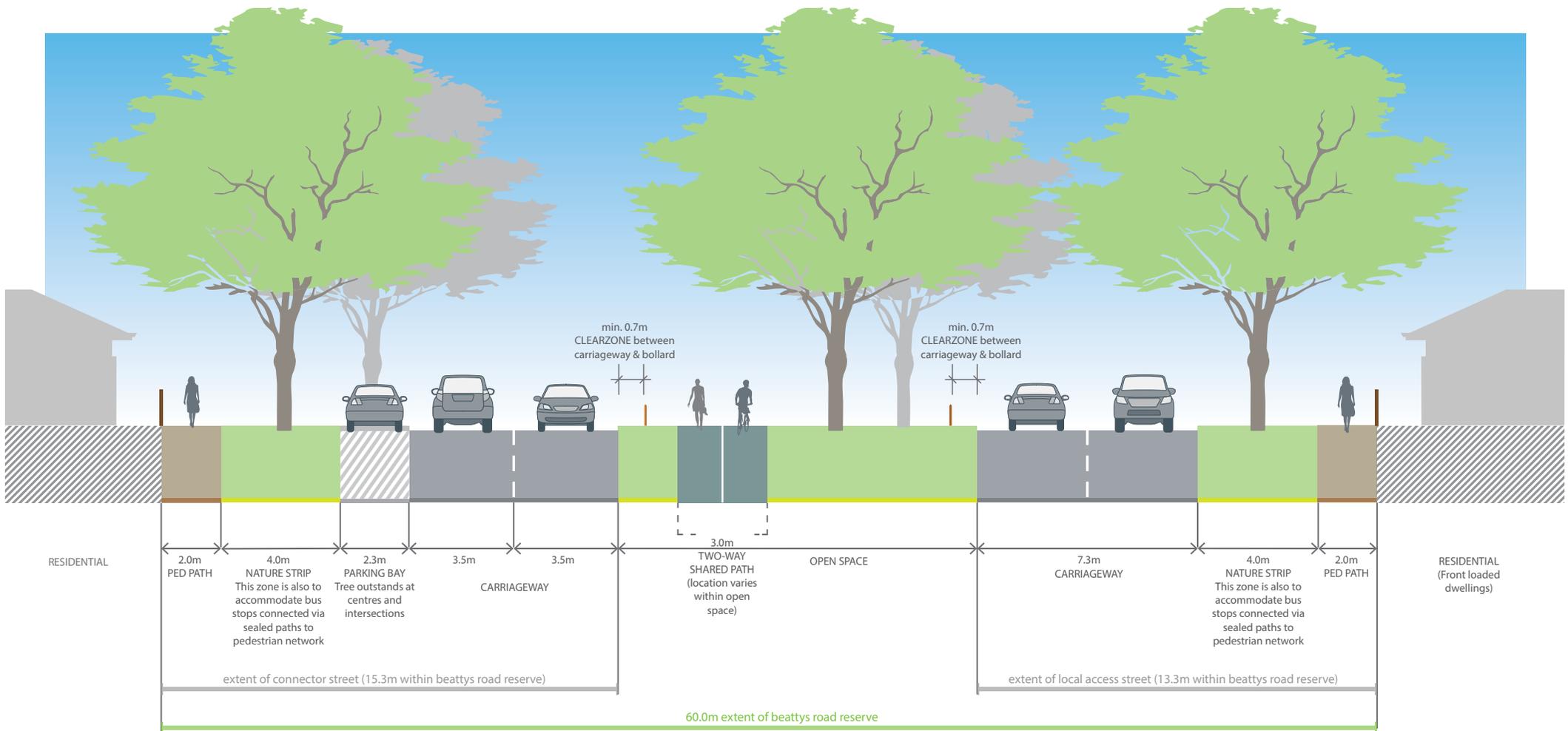
NOTES:

- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m.

Section 19a

Beattys Road Reserve

Connector Street (15.3m) & Direct Frontage (Rear or Side Load Dwellings)



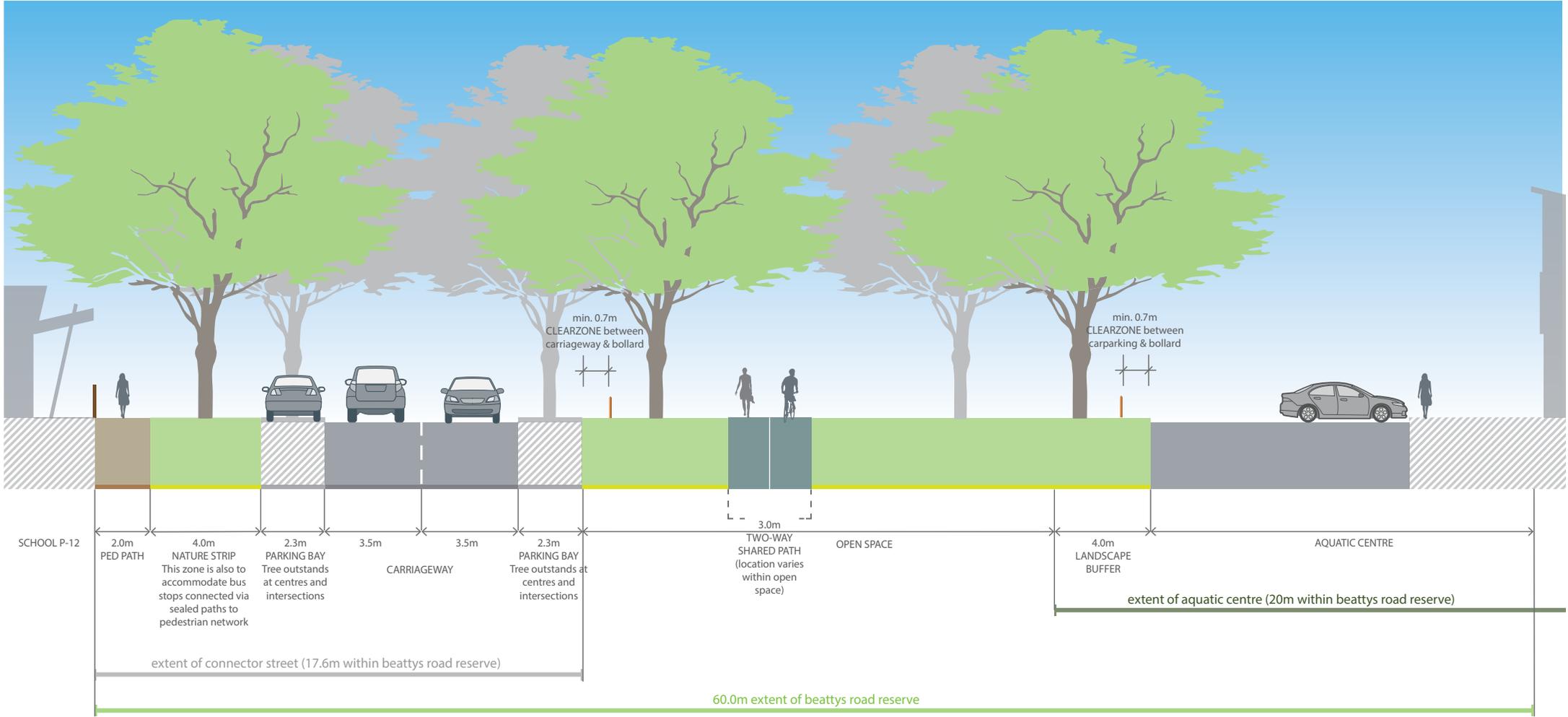
NOTES:

- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m.

Section 19b

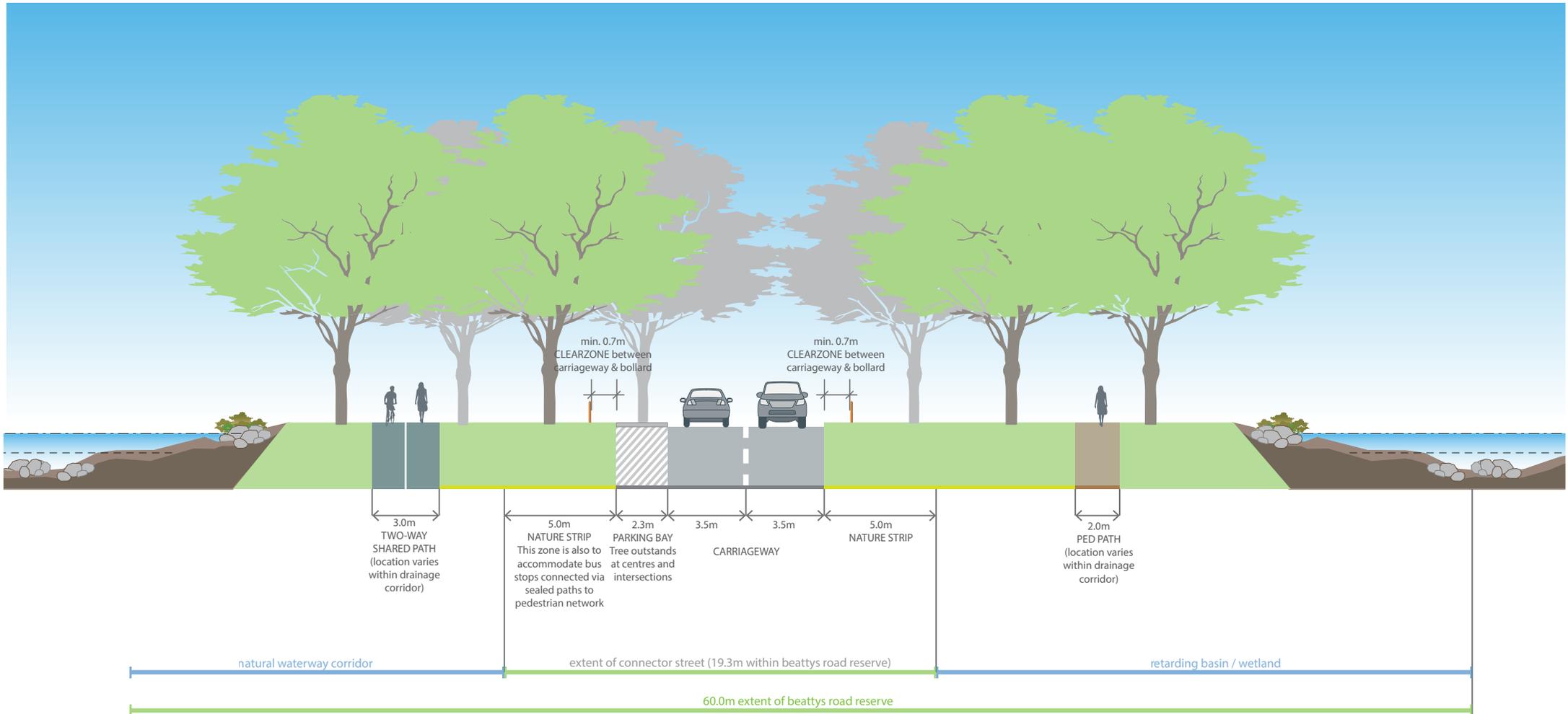
Beattys Road Reserve

Connector Street (15.3m) with Front Loaded Dwellings (13.3m) (Level 1 Access Street/Loop Road)



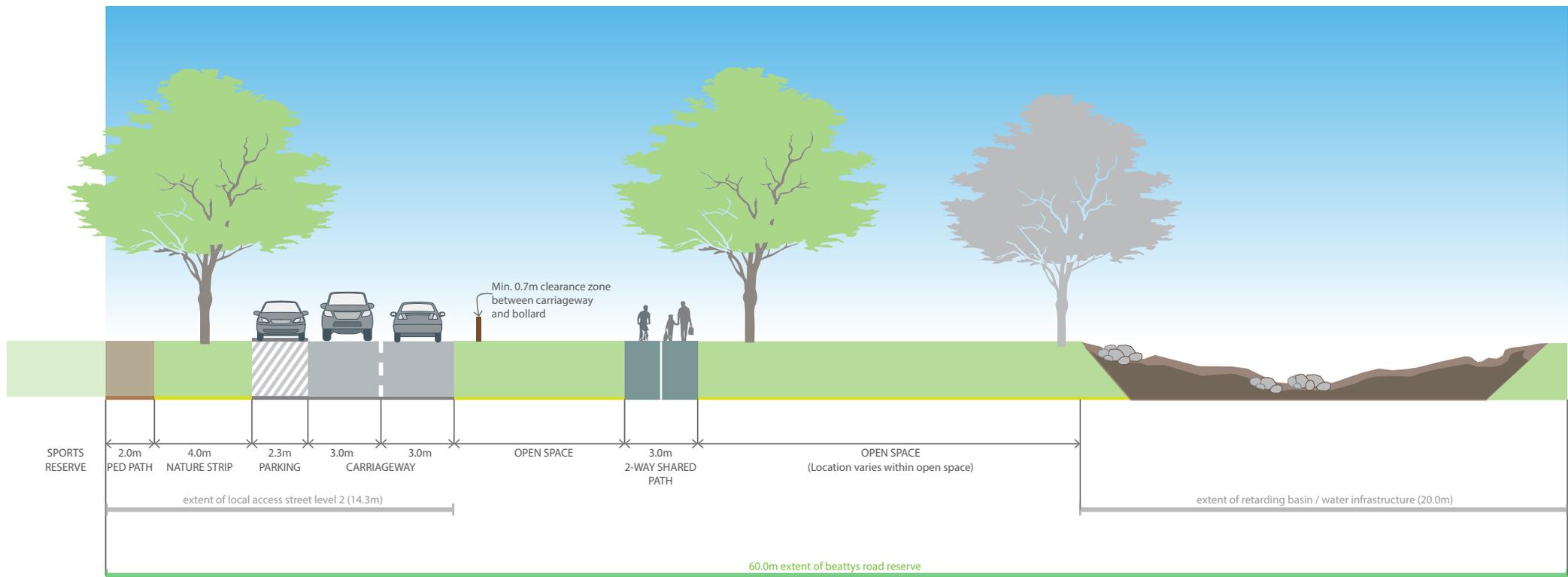
**NOTES:**

- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m.



NOTES:

- Retarding basin and embankment structures are separate from connector road construction
- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m.



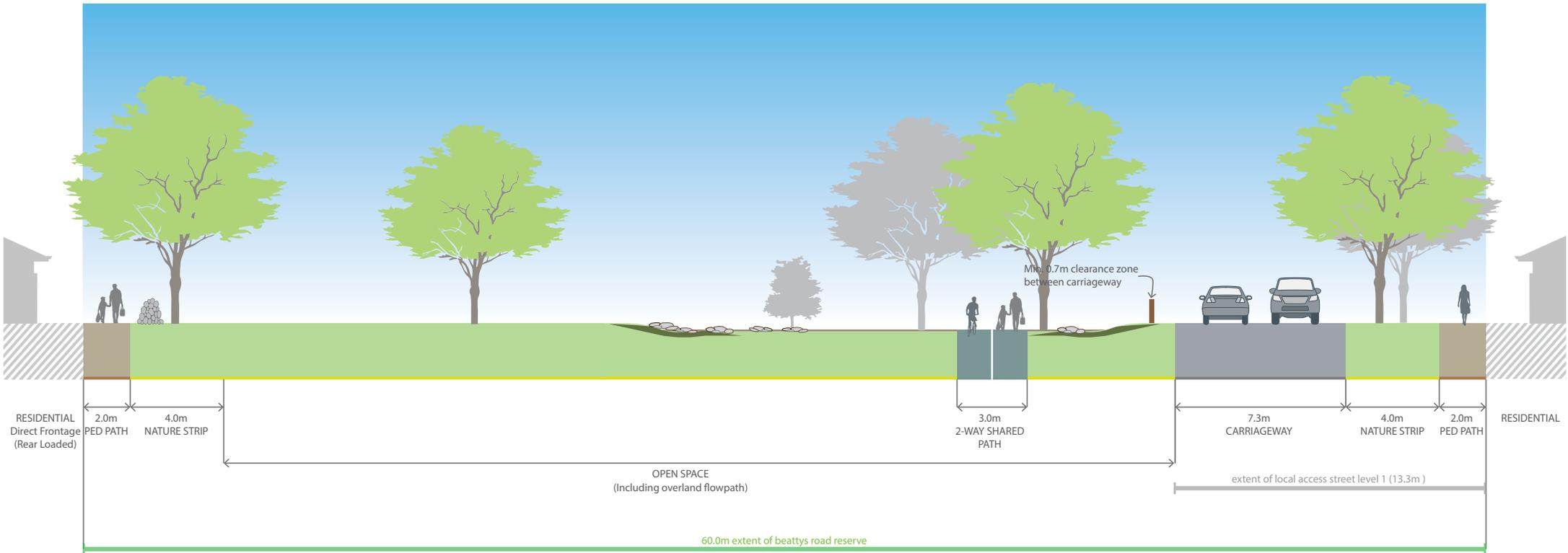
NOTES:

- Retarding basin and embankment structures are separate from connector road construction
- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m.

Section 22

Beattys Road Reserve

124 Access Street Level 2 (14.3m) & Retarding Basin / Wetland



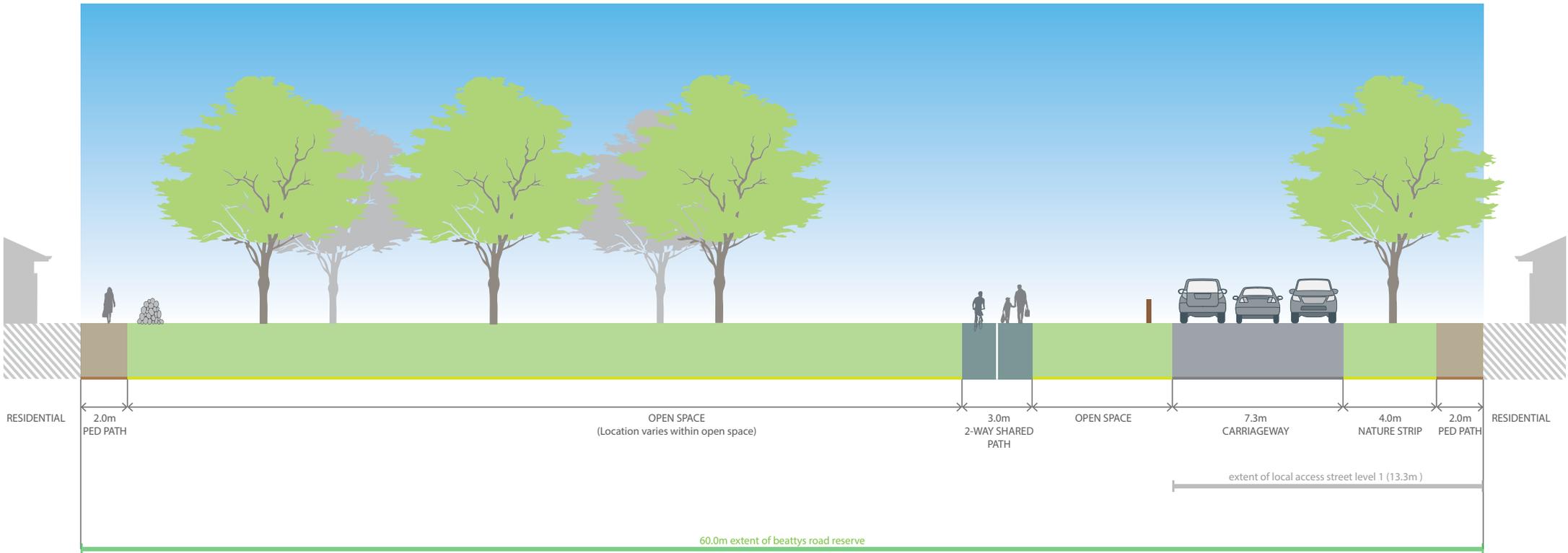
NOTES:

- Retarding basin and embankment structures are separate from connector road construction
- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m.

Section 23

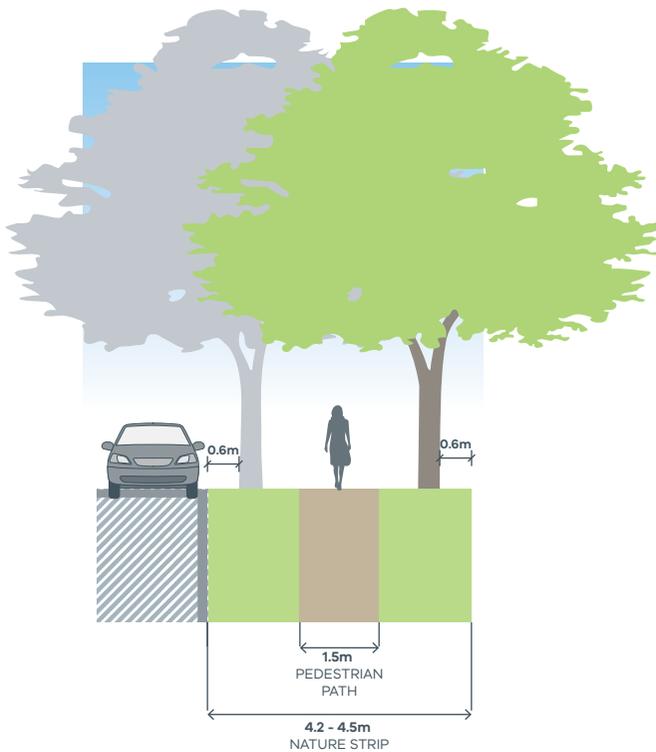
Beattys Road Reserve

Access Street Level 1 (13.3m) with Direct Frontage (Rear Loaded Dwellings) and Overland Flow Path

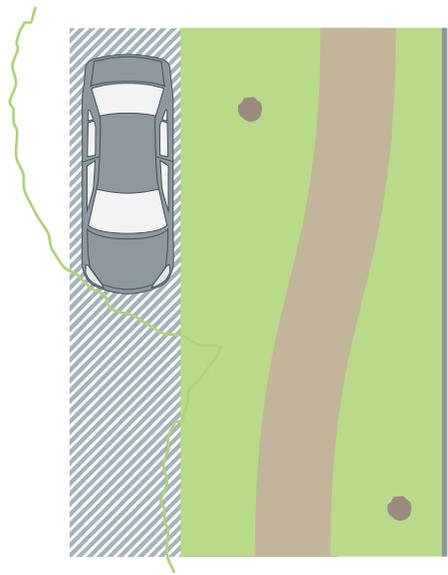


**NOTES:**

- Retarding basin and embankment structures are separate from connector road construction
- Provide bollards / low fence to deter vehicles from Beattys Road Reserve park
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Tree outstands must meet a maximum interval of 100m.

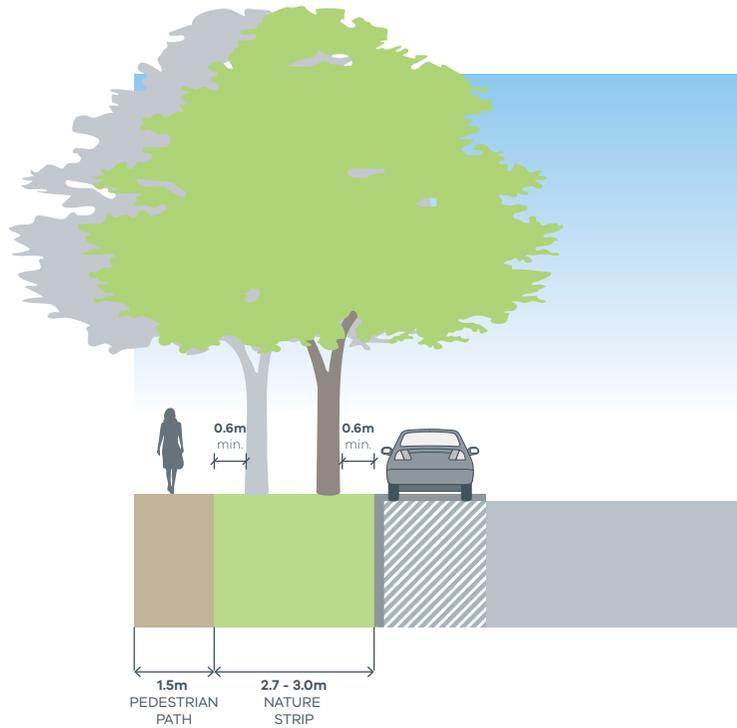


## Alternative Road Cross Sections



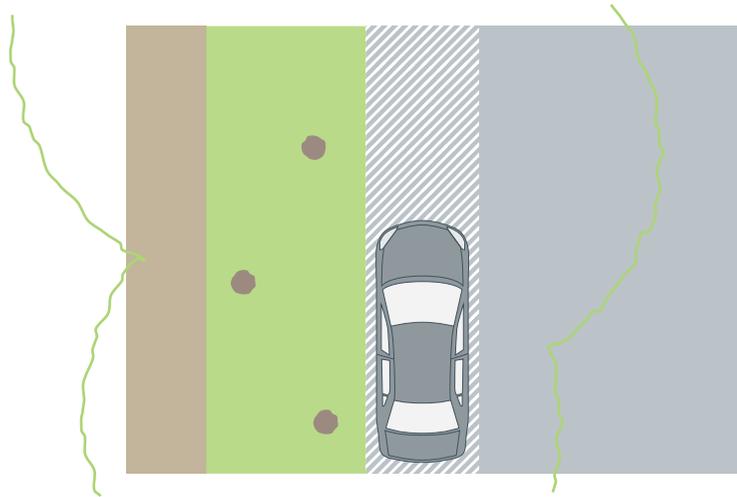
### NOTES:

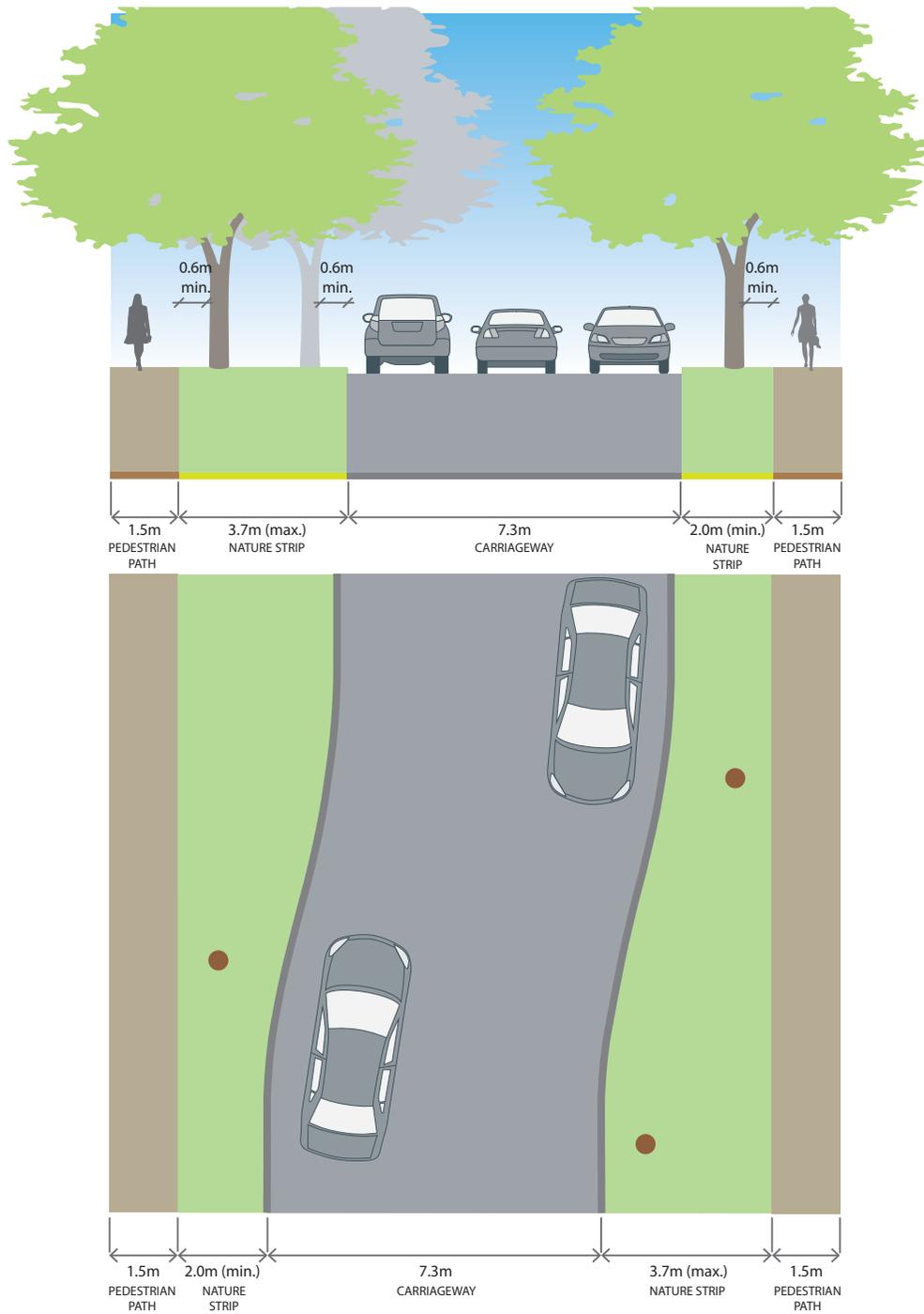
- Footpath in varying locations in nature strip
- Tree placement adjusts in response to footpath location
- Mature tree offset of footpath 1.0m from back of kerb and 0.6m from tree trunks
- Design of meandering footpath is to consider bin placement on nature strips, access to letter boxes for mail delivery, interface with driveways, definition of front allotment boundary and accommodation of bus stops.
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority.



NOTES:

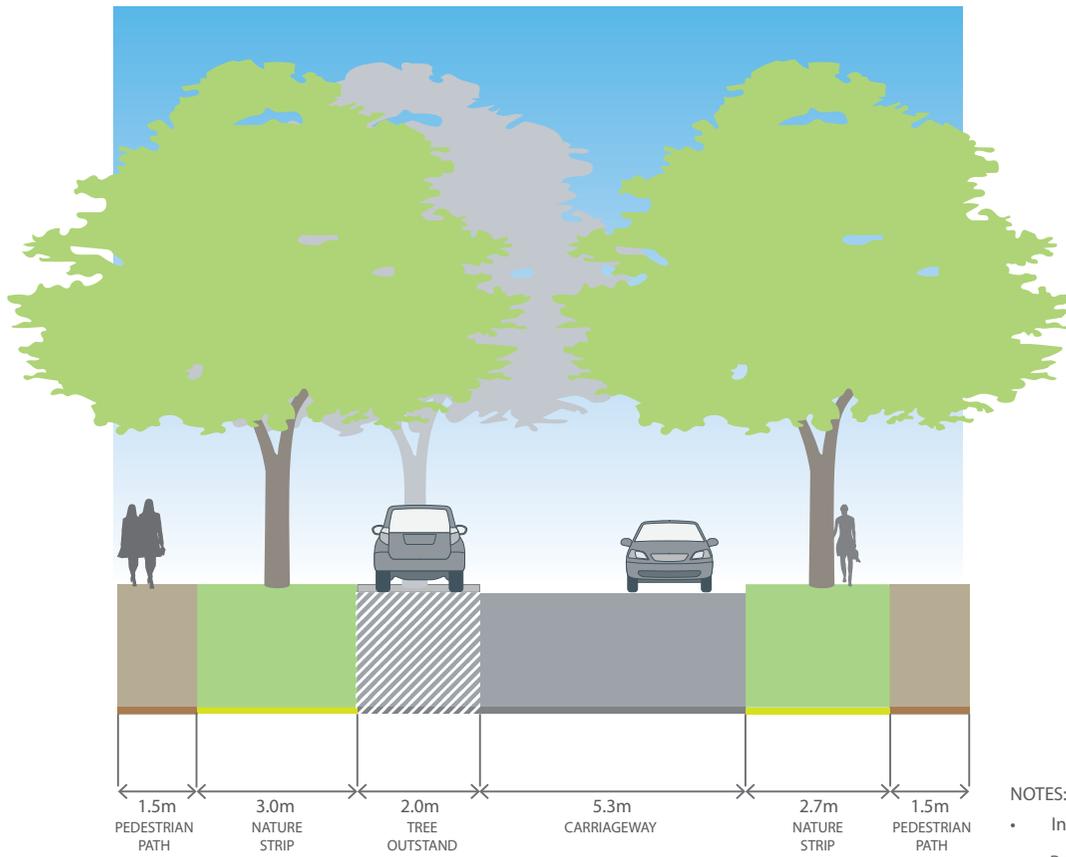
- Tree planting in varying locations in nature strip, in groups or clusters
- Minimum offset of tree trunks 0.6m from back of kerb and footpath edge





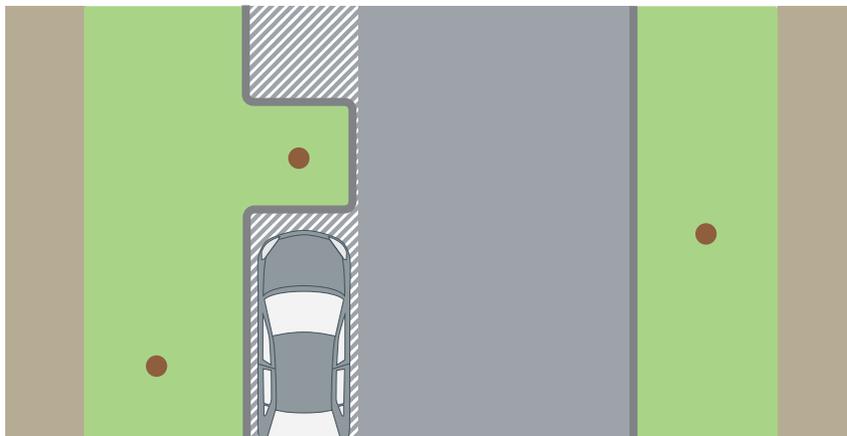
NOTES:

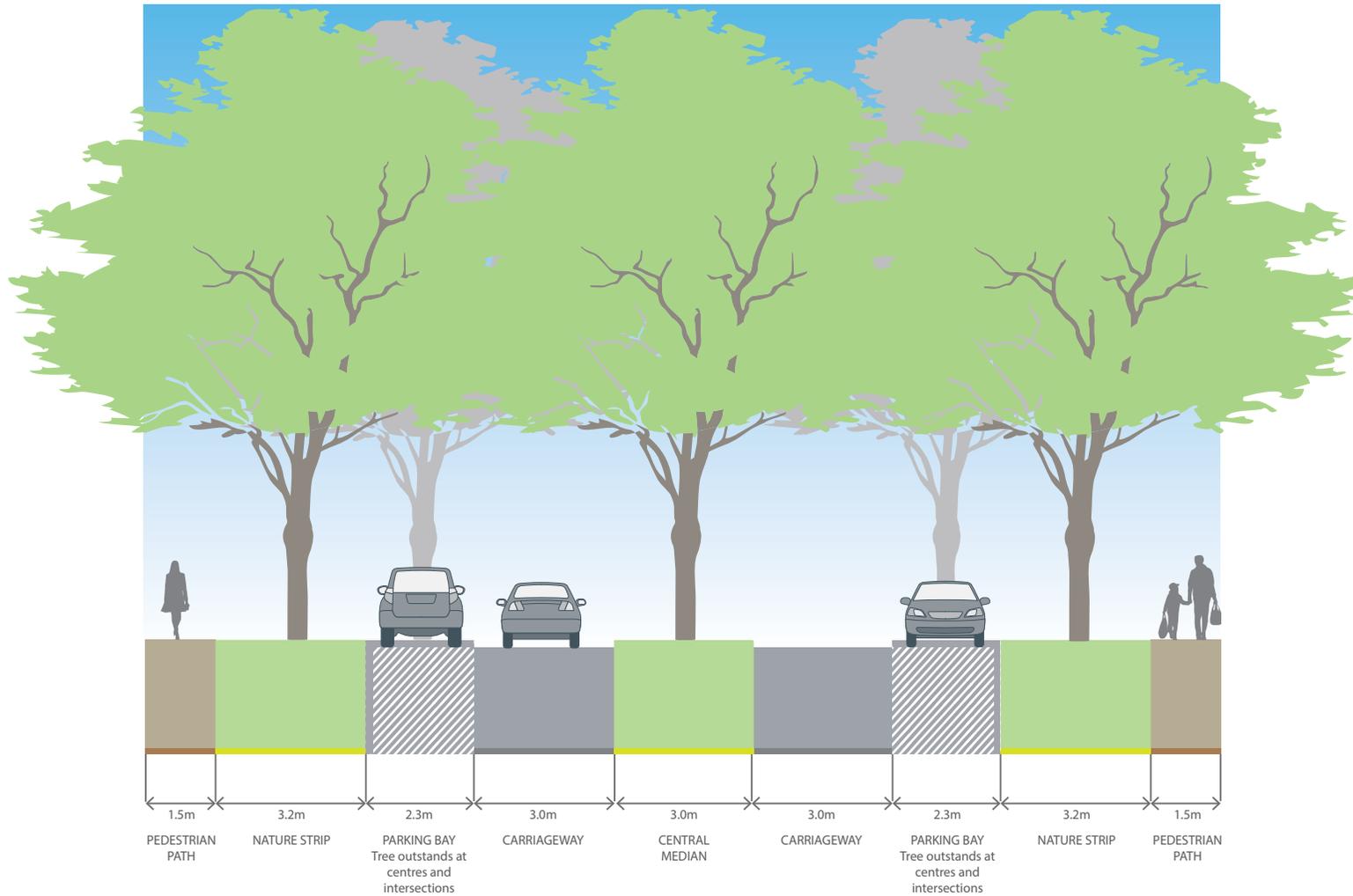
- Varying carriageway placement in road reserve
- Tree placement adjusts in response to carriageway location



NOTES:

- Include tree outstands at approx 50 – 100m centres on one side only
- Road design to ensure passage of emergency vehicles is accommodated
- Functional layout of the kerb outstands to be to the satisfaction of the responsible authority

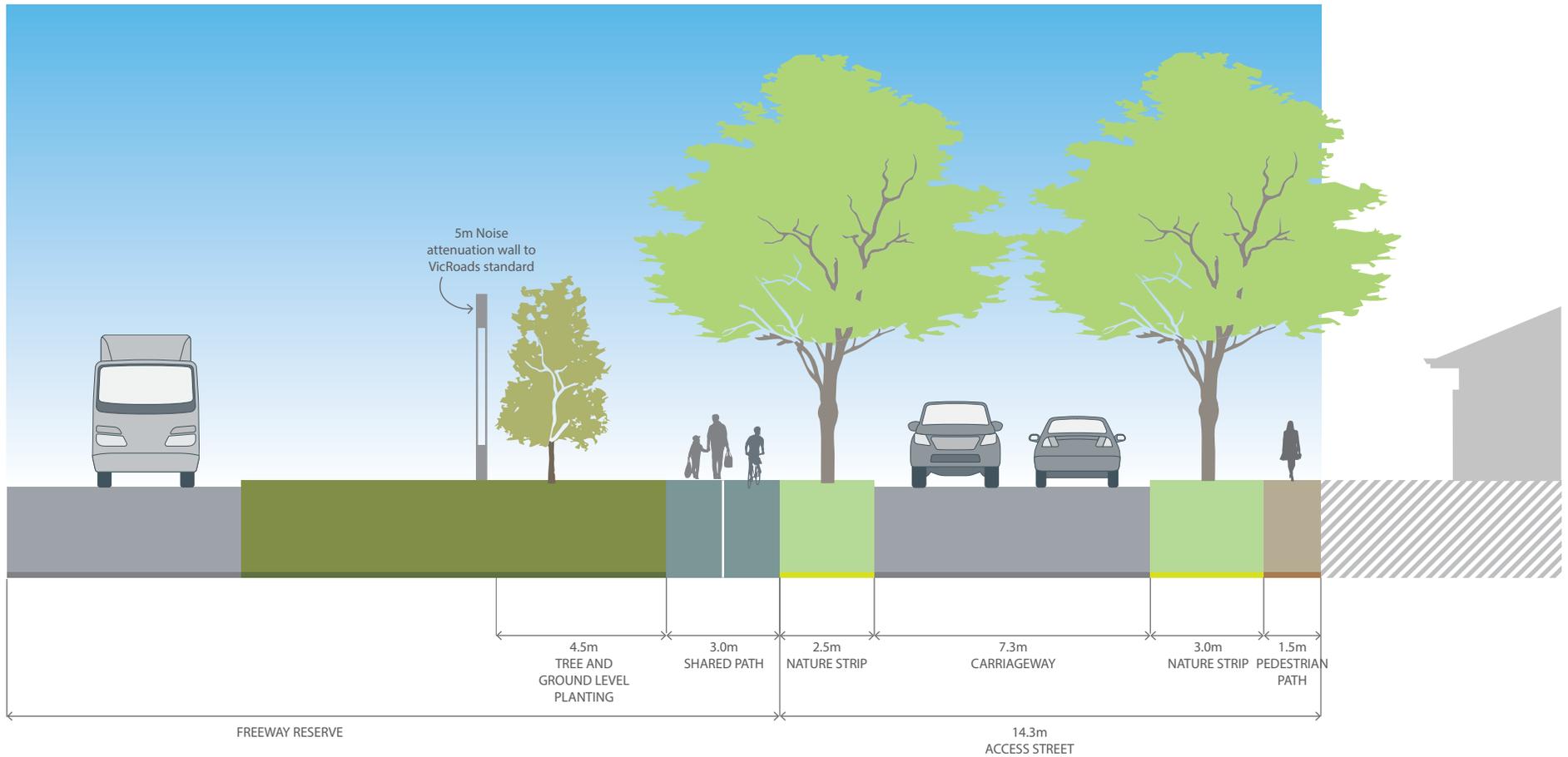




NOTES:

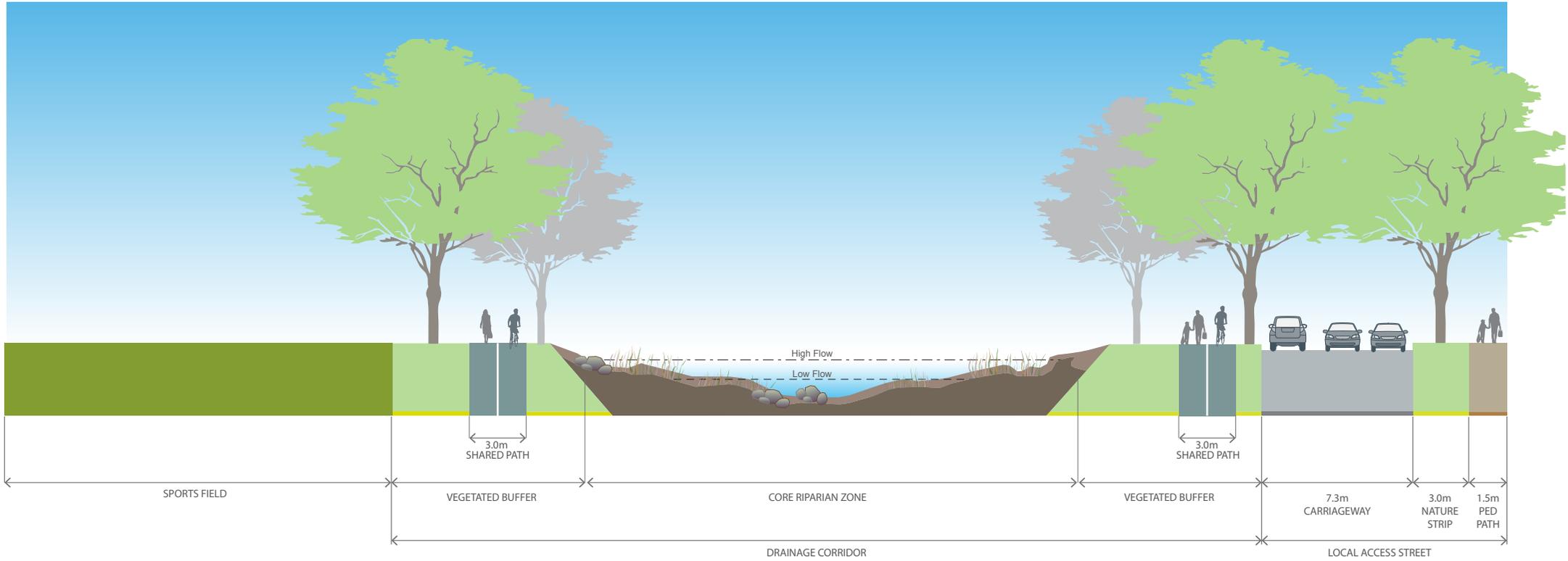
- Include a central median with canopy trees to create a boulevard effect
- Depending on the location of breaks in the median, provide intermediate pedestrian crossing points to accommodate mid-block crossings
- An alternative boulevard treatment can be achieved through a wider verge on one side capable of accommodating a double row of canopy trees
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb.

# Outer Metro Ring Road Interface Cross Section



**NOTES:**

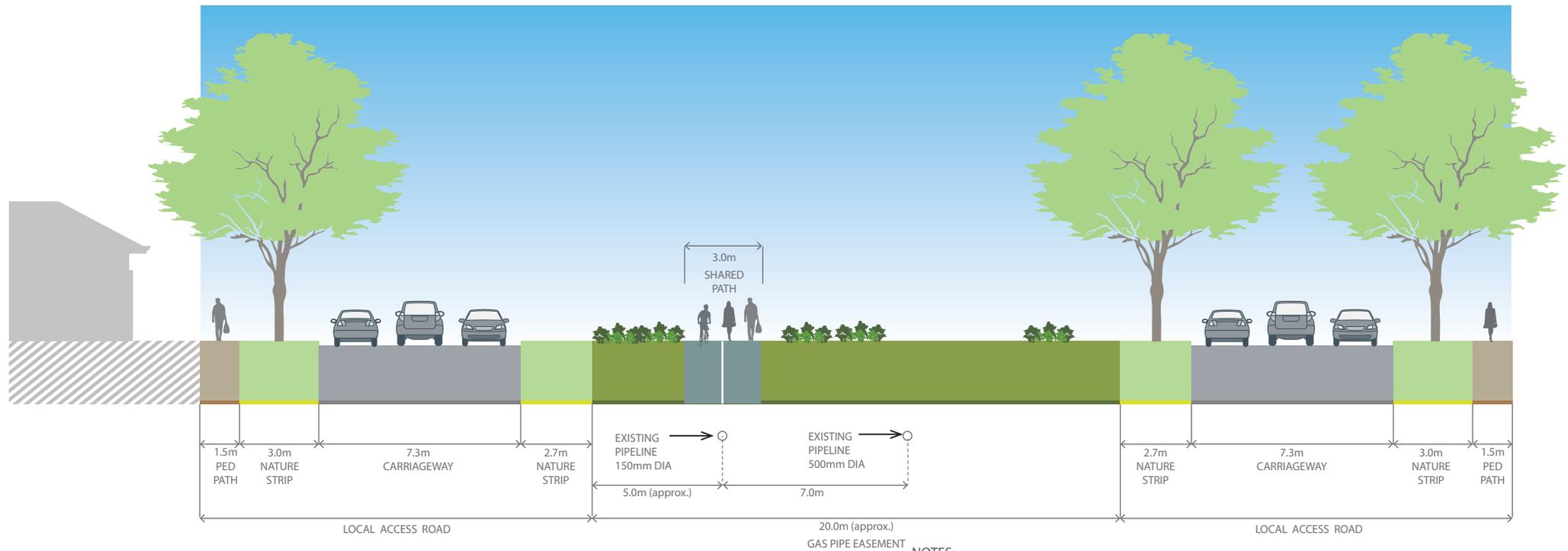
- OMR wall should be delivered by Vicroads
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority
- Shared paths to be delivered as developer works.



NOTES:

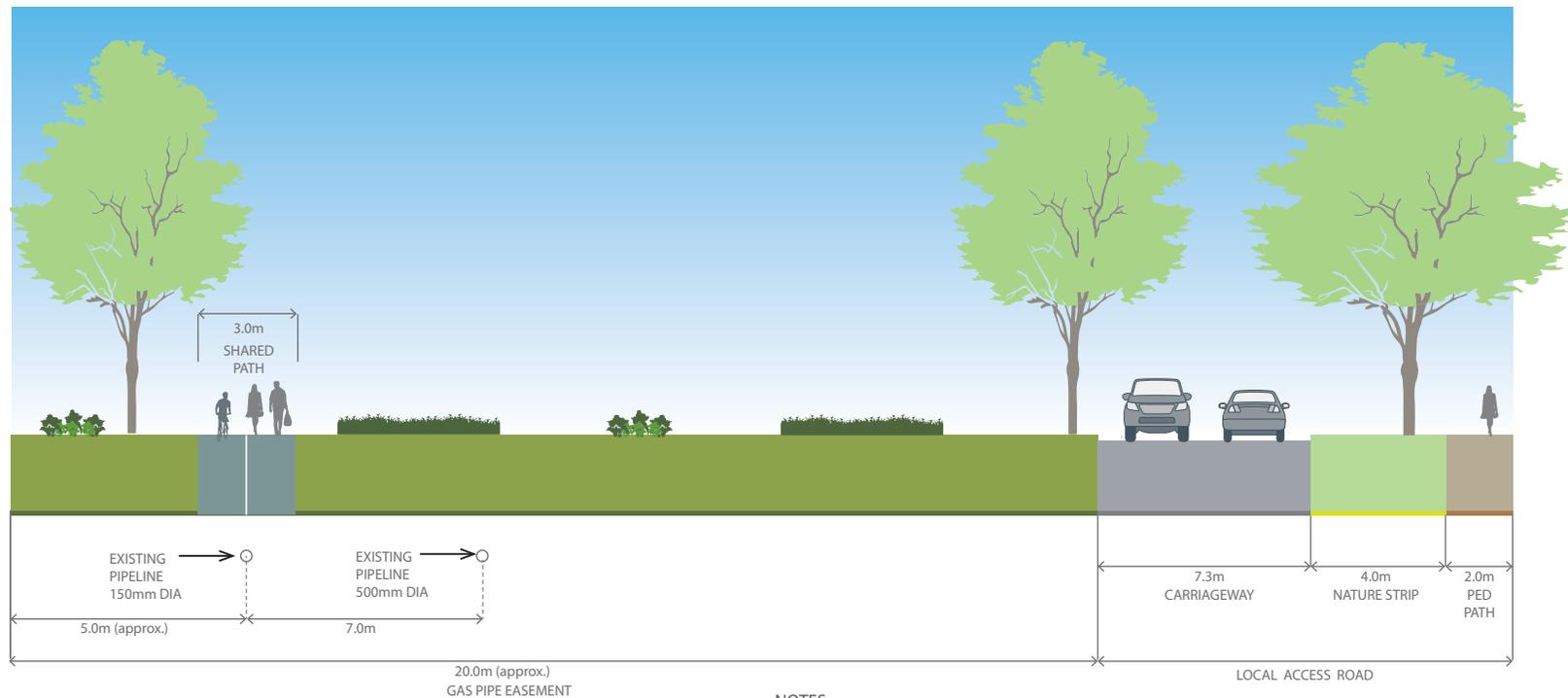
- Waterway widths subject to Melbourne Water approval
- Shared path placement is shown for both sports field and local access street interfaces for indicative purposes. The shared path network is shown on Plan 9
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority and relevant service authority
- Residential lots may directly face or side onto the drainage corridor with vehicular access to the lots provided from rear lanes and roads.

## Appendix F Easement Cross Sections



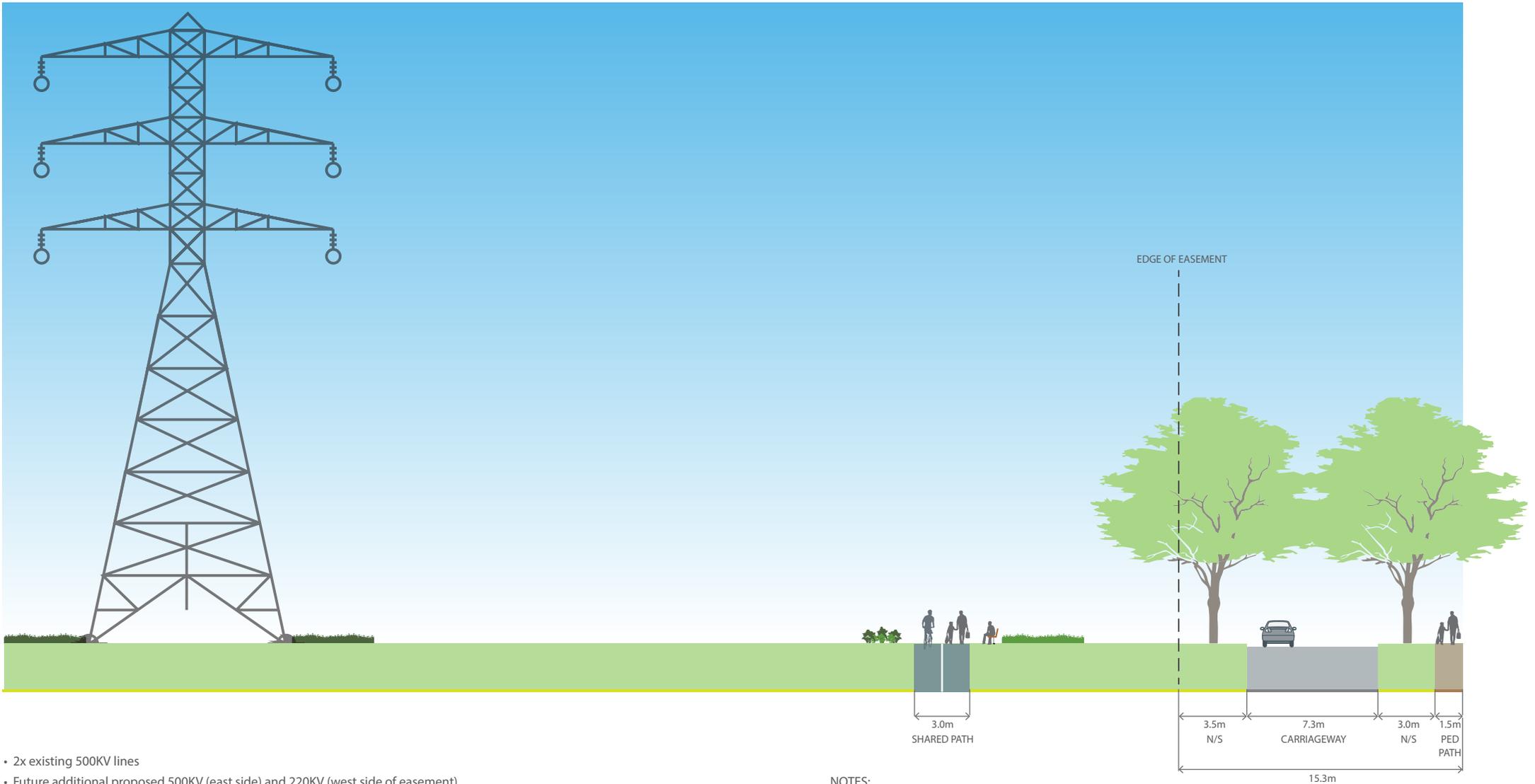
### NOTES:

- Location of pipelines is indicative only. Approval must be sought from APA prior to any works in the gas easement
- Indigenous shrubs and plants should be used in gas easement
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority and relevant service authority
- Hard landscaping (e.g. street furniture) and small trees may be included, provided sightlines between signs indicating the location of the pipe are not obscured and in compliance with mandated pipeline clearances is achieved
- Residential lots may directly face or side onto the pipeline easement with vehicular access to lots provided from rear lanes or roads.



**NOTES:**

- Location of pipelines is indicative only. Approval must be sought from APA prior to any works in the gas easement
- Indigenous shrubs and plants should be used in gas easement
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority and relevant service authority
- Hard landscaping (e.g. street furniture) and small trees may be included, provided sightlines between signs indicating the location of the pipe are not obscured and in compliance with mandated pipeline clearances is achieved
- Residential lots may directly face or side onto the pipeline easement with vehicular access to lots provided from rear lanes or roads.



- 2x existing 500KV lines
- Future additional proposed 500KV (east side) and 220KV (west side of easement).

NOTES:

- Easement uses vary; refer power lines easement table possible use and development
- Indigenous shrubs and plants should be used
- Part local access street may be provided within easement subject to easement owners approval
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space or easement with the consent of the responsible authority.



- 2x existing 500KV lines
- Future additional proposed 500KV (east side) and 220KV (west side of easement).

NOTES:

- Easement uses vary; refer power lines easement table possible use and development
- Indigenous shrubs and plants should be used
- Part local access street may be provided within easement subject to easement owners approval
- Mature street tree size must be in accordance with Melton City Council's landscaping policy
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas
- Verge widths may be reduced where roads abut open space or easement with the consent of the responsible authority.

**Appendix G** Beattys Road Reserve Delivery Responsibility Table and Concept Plan

Refer to Figures 1 – 4 and Figure 9 Example of Beattys Road Responsibility Plan

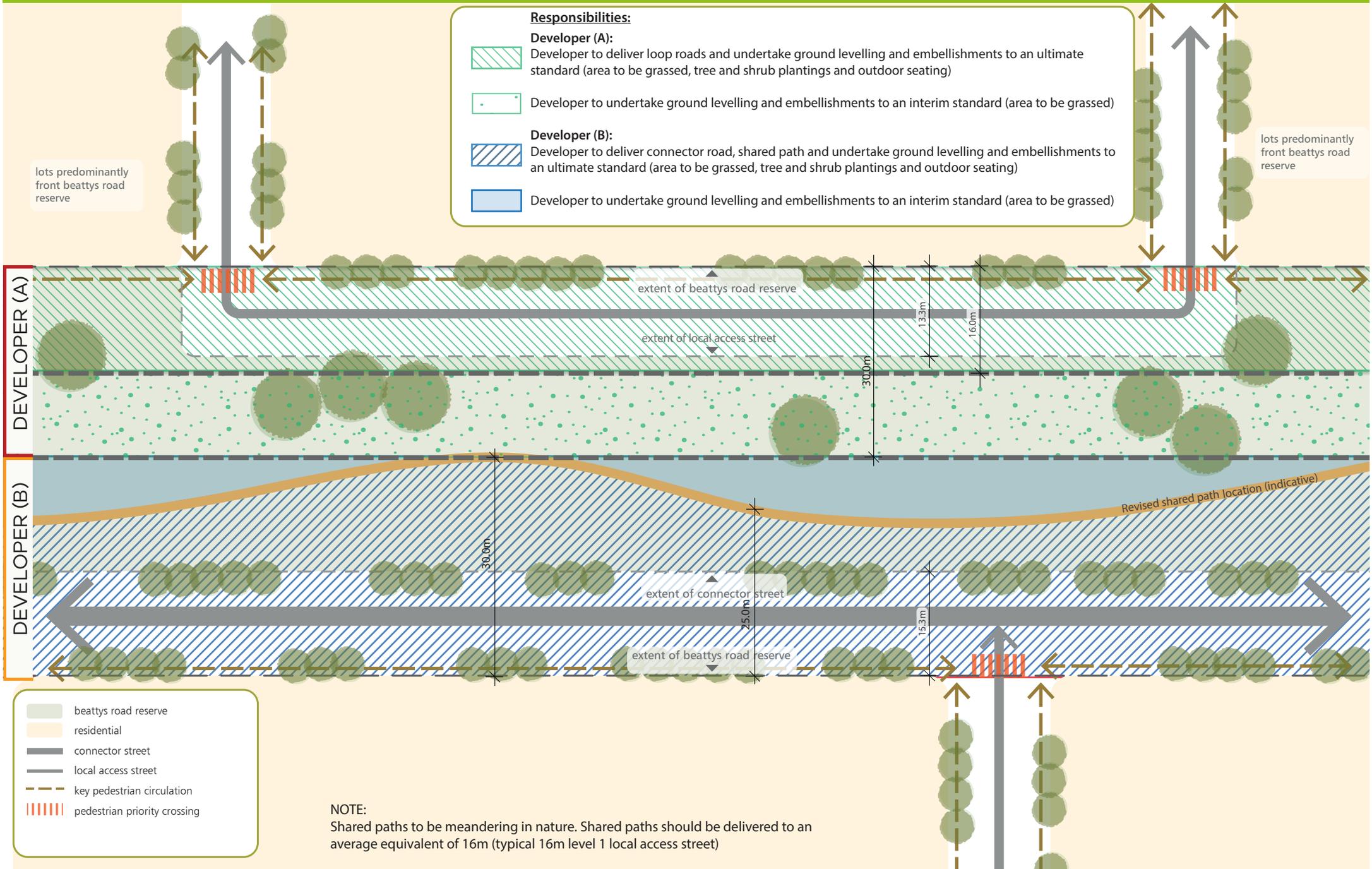
**Table 9** Beattys Road Reserve – delivery responsibility

PSP Parcel ID	Requirements as part of developer works				Infrastructure required in the PSP
	Construction of a street and/or overland path within Beattys Road	Extent of landscaping to 'ultimate standard' – Provide ground levelling, grass, trees, shrub plantings (and outdoor seating or other street furniture) required by responsible authority (R.A.) (May include local street/ loop road/ connector street with footpath on one side only as per Plan 3 or as required)	Extent of landscaping to 'interim standard' – Provide ground levelling and grass	Meandering shared path within Beattys Road	
8	N/A	N/A	N/A	No	N/A
9	N/A	From edge Beattys Road Reserve to 16m from edge (As 16m is the typical width for a loop road/local access street level 1)	From 16m line to 30m Beattys Rd centreline	Yes	N/A
10 (Area 1)	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	Yes	N/A
10 (Area 2)	<ul style="list-style-type: none"> <li>Local access 2</li> <li>Overland flow path</li> </ul>	From edge Beattys Road Reserve to 20m from edge (As 20m is the typical width for a local access street level 2)	From 20m line to 30m Beattys Road centreline	Yes	N/A
11 (Area 1)	N/A	N/A	From retarding basin to 30m Beattys Road centreline	No	Part retarding basin is in Beattys Road reserve
11 (Area 2)	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	Hopkins Road/ Tarletons Road intersection
12 (Area 1) - MTC	<ul style="list-style-type: none"> <li>N-S connector street to centreline of Beattys Road</li> </ul>	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	Yes	N/A
12 (Area 2) - community centre	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	Yes	N/A

PSP Parcel ID	Requirements as part of developer works				Infrastructure required in the PSP
	Construction of a street and/or overland path within Beattys Road	Extent of landscaping to 'ultimate standard' – Provide ground levelling, grass, trees, shrub plantings (and outdoor seating or other street furniture) required by responsible authority (R.A.)) (May include local street/ loop road/ connector street with footpath on one side only as per Plan 3 or as required)	Extent of landscaping to 'interim standard' – Provide ground levelling and grass	Meandering shared path within Beattys Road	
12 (Area 3) - retarding basin	<ul style="list-style-type: none"> <li>Connector street from north side Beattys Road Reserve to join Property 30 connector street</li> </ul>	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	Waterway
12 (Area 4) - residential	<ul style="list-style-type: none"> <li>Local access street to join connector street in Beattys Road reserve</li> </ul>	From edge Beattys Road reserve to 16m from edge	From 16m line to 30m Beattys Road centreline	No	N/A
12 (Area 5) - aquatic centre	N/A	The area occupied by the aquatic centre is to be embellished by Council to ultimate standard	From edge of aquatic centre to 30m Beattys Road centreline	No	Part aquatic centre located in Beattys Road reserve
12 (Area 6) - residential	<ul style="list-style-type: none"> <li>N-S connector street to centreline of Beattys Road reserve</li> </ul>	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	N/A
21	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	N/A
22	<ul style="list-style-type: none"> <li>E-W connector street connecting to property 28 connector street</li> </ul>	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	N/A
23	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	N/A
24	<ul style="list-style-type: none"> <li>N-S connector street to centreline of Beattys Road</li> </ul>	<ul style="list-style-type: none"> <li>From edge Beattys Road Reserve to 16m from edge</li> <li>The area occupied by the sports reserve is to be embellished by Council to ultimate standard</li> </ul>	From 16m line to 30m Beattys Rd centreline	No	Part sports reserve is in Beattys Road Reserve
25	<ul style="list-style-type: none"> <li>N/A</li> </ul>	<ul style="list-style-type: none"> <li>From edge Beattys Road Reserve to 16m from edge</li> <li>The area occupied by the sports reserve is to be embellished by Council to ultimate standard</li> </ul>	From 16m line to 30m Beattys Rd centreline	No	Part sports reserve is in Beattys Road Reserve

PSP Parcel ID	Requirements as part of developer works				Infrastructure required in the PSP
	Construction of a street and/or overland path within Beattys Road	Extent of landscaping to 'ultimate standard' – Provide ground levelling, grass, trees, shrub plantings (and outdoor seating or other street furniture) required by responsible authority (R.A.) (May include local street/ loop road/ connector street with footpath on one side only as per Plan 3 or as required)	Extent of landscaping to 'interim standard' - Provide ground levelling and grass	Meandering shared path within Beattys Road	
26	<ul style="list-style-type: none"> <li>Connector street including on road bike lane is delivered as part of Taylors Hill West DCP</li> </ul>	From edge Beattys Road Reserve to 25m from edge (As 25m is the typical width for a local access street level 2)	From 25m line to 30m Beattys Rd centreline	Yes	N/A
27	<ul style="list-style-type: none"> <li>Connector street</li> <li>N-S connector street to centreline of Beattys Road</li> </ul>	From edge Beattys Road Reserve to 25m from edge	From 25m line to 30m Beattys Rd centreline	Yes	N/A
28 (Area 1)	<ul style="list-style-type: none"> <li>Connector street connecting to property 12 connector street</li> </ul>	From edge Beattys Road Reserve to 25m from edge	From 25m line to 30m	Yes	N/A
28 (Area 2)	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	Yes	N/A
28 (Area 3)	<ul style="list-style-type: none"> <li>Connector street (including to northern edge Beattys Road reserve)</li> </ul>	From edge Beattys Road Reserve to 25m from edge	From 25m line to 30m Beattys Rd centreline	Yes	N/A
29	<ul style="list-style-type: none"> <li>Connector street</li> </ul>	From edge Beattys Road Reserve to 25m from edge	From 25m line to 30m Beattys Rd centreline	Yes	N/A
30	<ul style="list-style-type: none"> <li>Connector street</li> </ul>	From edge Beattys Road Reserve to 25m from edge	From 25m line to 30m Beattys Rd centreline	Yes	N/A
31	<ul style="list-style-type: none"> <li>Local access 1 to join connector road on north side of Beattys Road Reserve</li> </ul>	From edge Beattys Road Reserve to 25m from edge (for connector street)	From 25m line to 30m Beattys Rd centreline	Yes	Waterway
32	<ul style="list-style-type: none"> <li>N-S connector street to centreline of Beattys Road</li> </ul>	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	N/A
33	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	Hopkins Road/ Tarletons Road intersection

PSP Parcel ID	Requirements as part of developer works				Infrastructure required in the PSP
	Construction of a street and/or overland path within Beattys Road	Extent of landscaping to 'ultimate standard' – Provide ground levelling, grass, trees, shrub plantings (and outdoor seating or other street furniture) required by responsible authority (R.A.)) (May include local street/ loop road/ connector street with footpath on one side only as per Plan 3 or as required)	Extent of landscaping to 'interim standard' - Provide ground levelling and grass	Meandering shared path within Beattys Road	
34	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	Yes	N/A
35	<ul style="list-style-type: none"> <li>Local access 2 to centreline of Beattys Road Reserve</li> </ul>	From edge Beattys Road Reserve to 20m from edge	From 20m line to 30m Beattys Rd centreline	Yes	N/A
39	<ul style="list-style-type: none"> <li>Local access 1 to centreline of Beattys Road Reserve</li> <li>N-S component of overland flow path</li> </ul>	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	N/A
40	N/A	From edge Beattys Road Reserve to 16m from edge	From 16m line to 30m Beattys Rd centreline	No	N/A



## Appendix H Service placement guidelines

### Standard road cross sections

The Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011) outlines placement of services for a typical residential street environment. This approach is appropriate for the majority of the 'standard' road cross sections outlined in Appendix D containing grassed nature strips, footpaths and road pavements.

### Non-standard road cross sections

To achieve greater diversity of streetscape outcomes, which enhances character and amenity of these new urban areas, non-standard road cross sections are also required. Non-standard road cross sections will be necessary to address local needs, such as fully sealed verges for high pedestrian traffic areas in town centres and opposite schools. This PSP contains suggested non-standard 'variation' road cross sections in Appendix D, however other non-standard outcomes are encouraged.

For non-standard road cross sections where service placement guidance outlined in the Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011) is not applicable, the following service placement guidelines will apply.

**TABLE NOTES**

1. Trees are not to be placed directly over property service connections
2. Placement of services under road pavement is to be considered when service cannot be accommodated elsewhere in road reserve. Placement of services beneath edge of road pavement/parking bays is preferable to within traffic lanes
3. Where allotment size/frontage width allows adequate room to access and work on a pipe
4. Where connections to properties are within a pit in the pedestrian pavement/footpath

	UNDER PEDESTRIAN PAVEMENT	UNDER NATURE STRIPS	DIRECTLY UNDER TREES <sup>1</sup>	UNDER KERB	UNDER ROAD PAVEMENT <sup>2</sup>	WITHIN ALOTMENTS	NOTES
<b>SEWER</b>	Possible	Preferred	Possible	No	No	Possible <sup>3</sup>	
<b>POTABLE WATER</b>	Possible <sup>4</sup>	Preferred	Possible	No	No	No	Can be placed in combined trench with gas
<b>RECYCLED WATER</b>	Possible <sup>4</sup>	Preferred	Possible	No	No	No	
<b>GAS</b>	Possible <sup>4</sup>	Preferred	Preferred	No	No	No	Can be placed in combined trench with potable water
<b>ELECTRICITY</b>	Preferred <sup>4</sup>	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
<b>FTTH/ TELCO</b>	Preferred <sup>4</sup>	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
<b>DRAINAGE</b>	Possible	Possible	Possible	Preferred	Possible	Possible <sup>3</sup>	
<b>TRUNK SERVICES</b>	Possible	Possible	Possible	Possible	Possible	No	

**General principles for service placement**

- Place gas and water on one side of road, electricity on the opposite side
- Place water supply on the high side of road
- Place services that need connection to adjacent properties closer to these properties
- Place trunk services further away from adjacent properties
- Place services that relate to the road carriageway (eg. drainage, street light electricity supply) closer to the road carriageway
- Maintain appropriate services clearances and overlap these clearances wherever possible
- Services must be placed outside of natural waterway corridors or on the outer edges of these corridors to avoid disturbance to existing waterway values.

## Appendix I Open space delivery guidelines

### PARK HIERARCHY

The open space network is made up of a diverse range of spaces which will vary in sizes, shape and function. The hierarchy outlined below provides information and guidance on the key open space categories listed in of this PSP and what role and function they generally have in the network.

#### Pocket Parks (<0.2Ha)

These parks are small more intimate spaces that can provide incidental and spontaneous recreation and relaxation such as sitting, resting and eating lunch within a short safe walking distance of residents and workers. In town centres and built up areas they may incorporate significant hard and / or high standard soft landscaping to accommodate more intensive use.

Pocket parks will also complement the role of local parks and may sometimes be designed to have a local park role (including a play space), again often when associated with built up areas.

Facilities will generally be tailored to support a stay length of less than ½ an hour.

#### Neighbourhood Parks (0.2-1Ha) (Defined as Local 0.2- 2Ha in Melton City Council's Open Space Plan 2016-2026)

Typically small to medium in size parks that primarily provide opportunities for informal and opportunistic recreation, relaxation or play to local residents within short safe walking distance. Such reserves typically include basic facilities such as seats, walking paths and a small playground that support stay lengths up to one hour.

Near town centres and built up areas, the role, function and importance of these spaces may increase and they may include more intensive infrastructure to support greater use. In this way, local parks can complement the role of pocket parks.

#### Community Parks (1-5Ha) (Defined as Local 0.2- 2Ha then District for 2Ha+ in Melton City Council's Open Space Plan 2016-2026)

Medium parks, often with more diverse facilities and landscape characteristics that supports a range of informal recreation, relaxation or play opportunities for short to medium time periods from 0.5-2hrs. Facilities for organised recreation may sometimes also be provided for. These parks service residents within a short to medium safe walking catchment and they are also the local park for local residents.

In built up areas, the role, function of importance of these spaces may increase and they may carry more intensive infrastructure to support greater use.

#### District Parks (5-15Ha) (Defined as District for 2Ha+ in Melton City Council's Open Space Plan 2016-2026)

Medium to large parks that serve a medium suburb scale catchment accessible via longer walks, short to medium cycle rides and short vehicle trips. Provision of facilities for organised sports will often be the focus of these parks, complemented by infrastructure for informal recreation such as playgrounds, picnic areas and walking / shared trails Infrastructure will support visits for longer periods Of 1-4hrs + including potentially staging of community events.

District parks are also the local neighbourhood and community park for local residents.

#### Municipal Parks (15-50Ha) (Defined as District Park for 2Ha+ then Regional for 20Ha + in Melton City Council's Open Space Plan 2016-2026)

Large to very large Council owned and / or managed parks that can accommodate high visitation from a broad municipal or greater catchment. Will often integrate a wide range of formal and informal functions and include facilities (such as car-parking, toilets, shelters and picnic facilities, walking trails and larger playgrounds) to support longer stays (1-4hrs+) multiple social gatherings and staging of large scale community events . Organised sporting infrastructure and / or significant natural features may also form a significant component of such reserves.

Municipal scale parks provided primarily for landscape and conservation values will likely have more low key infrastructure that supports lower impact informal and nature based recreation.

Municipal parks will also be the local, neighbourhood and district park for nearby residents.

#### Metropolitan Parks (50Ha+ +) (Defined as Regional for 20Ha + in Melton City Council's Open Space Plan 2016-2026)

Large to very large State owned and / or managed parks (usually via Parks Victoria) that accommodate and promote high visitation from a broad regional and / or metropolitan catchment. Metropolitan parks generally provide facilities for informal recreation in natural and / or semi natural settings and will often be associated with significant waterways and extensive areas of native, and / or historically important exotic vegetation. Infrastructure in these parks will usually include car-parking, toilets, shelters and picnic facilities, walking trails and larger playgrounds and even cafes to support longer stays, multiple social gatherings and staging of large scale community events. Organised sporting infrastructure may sometimes be strategically incorporated with these parks.

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Metropolitan scale parks (or parts thereof) provided primarily for conservation and biodiversity purposes will likely have more restricted access with lower impact infrastructure to support targeted low key informal and nature based recreation.

Municipal / regional parks will also be the local park for nearby residents.

### **Linear Parks**

Each of the above open space types (although less likely for pocket parks) may also have a linear or elongated design with a key function being to provide pedestrian and cyclist links between destinations in a parkland setting. Waterways and utilities easements will most often provide the backbone of the linear park system in a given area.

Linear parks may provide for neighbourhood, community, municipal or regional connectivity generally as follows:

#### **Neighbourhood**

Areas typically < 100m in length that provide a formal or informal link between the local street network and / or open space.

#### **Community**

Areas typically 100m - 1km in length that provide a formal or informal link within the wider neighbourhood street and open space network. Community linear parks can be comprised of a network of neighbourhood links.

#### **District**

Areas typically 1 - 5km in length that provide formal or informal linkages between districts and open space destinations. These areas can comprise a network of neighbourhood and / or community links.

#### **Municipal / Metropolitan**

Areas typically > 5km in length that provide formal or informal linkages at the municipality / metropolitan scale. These areas can encompass smaller links (neighbourhood/ community / regional).

### **Town Square/ Urban Park**

A passive recreation park providing opportunities for a variety of recreational and social activities in an urban setting. They are located predominantly in medium to high density residential area and mixed use centres or corridors. They provide an important role in meeting the passive recreation needs of residents, workers and visitors in activity centres and/or medium to high density residential areas.

Town squares are to be predominantly hard landscaped, while urban parks have less hardstand than town squares, but more than traditional neighbourhood passive recreation parks. Urban parks also offer the opportunity for low key kick and throw activities with a small turfed area.

Both parks are to integrate within their design a number of skate / scooter'able furniture pieces, rails, stairs, ledges, ramps and / or other 'plaza' type elements.

